

## AGENDA

CITY OF CEDAR FALLS, IOWA
PLANNING AND ZONING COMMISSION
WEDNESDAY, OCTOBER 10, 2018
5:30 PM AT CITY HALL - COUNCIL CHAMBERS

1. Call to Order and Roll Call
2. Approval of Minutes
3. Public Comments
4. New Business

A. HWY-1 District Site Plan Review - Fleet Farm Retail and Convenience Store $\quad$| Location: | SW Corner of Highway 58 and W. Ridgeway Avenue |
| ---: | :--- |
| Applicant: | Midland Atlantic Development Company, L.L.C. |

5. Old Business

## A. Park Ridge Estates Preliminary Plat (Discussion Deferred to October 24)

Location: 20.8 acre property at the north end of Lakeshore Drive Applicant: Larry Hill, owner; Wingert Development, CGA, Inc. Engineer Previous Discussion: September 12, 2018 Staff Recommendation: Discussion Deferred to October 24 meeting P\&Z Action Needed: None
6. Commission Updates
7. Adjournment

Reminders:

- October $24^{\text {th }}$ and November $14^{\text {th }}$ Planning \& Zoning Commission Meeting
- October $15^{\text {th }}$ and November $5^{\text {th }}$ City Council Meeting


# Cedar Falls Planning and Zoning Commission Regular Meeting September 12, 2018 <br> City Hall Council Chambers 220 Clay Street, Cedar Falls, Iowa 


#### Abstract

MINUTES The Cedar Falls Planning and Zoning Commission met in regular session on Wednesday, September 12, 2018 at 5:30 p.m. in the City Hall Council Chambers, 220 Clay Street, Cedar Falls, lowa. The following Commission members were present: Arntson, Giarusso, Hartley, Holst, Leeper, Saul and Wingert. Adkins and Oberle were absent. Karen Howard, Community Services Manager, David Sturch, Planner III and Iris Lehmann, Planner I, were also present. 1.) Acting Chair Holst noted the Minutes from the August 22, 2018 regular meeting are presented. Ms. Giarusso made a motion to approve the Minutes as presented. Mr. Hartley seconded the motion. The motion was approved unanimously with 7 ayes (Arntson, Giarusso, Hartley, Holst, Leeper, Saul and Wingert) and 0 nays. 2.) The first item of business was a continuation of public hearing regarding a rezoning request at the end of Lakeshore Drive from A-1, Agricultural to R-1, Residential. Acting Chair Holst introduced the item and Mr. Wingert noted that he will be abstaining from the item. Mr. Sturch provided background information, explaining that the item was introduced at the last meeting. It is a 20 acre parcel at the end of Lakeshore Drive that is proposed for residential use. He noted that utilities meet requirements, as well as the land use map. Staff recommends approval with conformance to all city staff recommendations and technical comments as well as any comments or direction from the Commission.


Tamie Stahl, 1009 Lakeshore Drive, stated that she was told when they moved into their home they were told this area would never be rezoned as there were too many issues and asked how it could be considered at this time.

Mr. Adam Daters, Clapsaddle Garber Associates, stated that he believes all criteria for rezoning have been met.

Ms. Saul noted that she feels that it is straightforward and consistent with the land use map. Mr. Holst agreed.

Mr. Leeper made a motion to approve. Mr. Arntson seconded the motion. The motion was approved unanimously with 5 ayes (Arntson, Giarusso, Hartley, Holst, Leeper and Saul), 2 abstentions (Holst and Wingert) and 0 nays.
3.) The next item for consideration by the Commission was the Park Ridge Estates Preliminary Plat. Acting Chair Holst introduced the item and Mr. Sturch provided background information. He explained that approximately 9 acres of the parcel is in the floodplain, it contains significant tree stands, steep slopes and meandering streams from the west and south to the Cedar River. He noted that the goal is to maintain those things, and that there is a required review for grading, storm water detention and runoff. The design is to create a controlled runoff for that area. Mr. Sturch displayed the proposed plat and identified the lots and easements, as well as detention basins. He explained the proposed plans for the runoff to divert into a ravine. Staff would like to discuss the plat at this time and gather any comments for continued discussion at the next Planning and Zoning meeting.

Mr. Adam Daters, Clapsaddle Garber Associates, came forward to clarify that they are in agreement with the additional requirements and their intent is to leave the area as natural as possible. Mr. Holst asked if there any consideration of a future extension of Lakeshore Drive. Mr. Daters noted that there wasn't any discussion on that. Mr. Leeper stated his concern for the storm water and asked if it might be better to release it into the stream versus a controlled release. Mr. Arntson asked about the elevation and drainage, and where the water will go. Mr. Daters provided answers to each inquiry.

Mr. Leeper asked if Mr. Daters is comfortable stating that this will not make things worse. Mr. Daters explained stated that they don't affect it currently and that it will not be worse.

Tamie Stahl, 1009 Lakeshore Drive, noted concerns about the development and said that the Lakeshore Development has not been treated well. She reiterated concerns with storm water management and showed the debris that come through the pond whenever there is a greater amount of water flow. She also passed out pictures of the issues in the area when it rains.

Lisa Sage, owner of the lot on corner of Lakeshore and Lilliput, asked for clarification about runoff from the south. Mr. Daters explained that they will be trying to push the water drainage further to the north.

Ms. Saul recommended that staff walk the property to get a better view as opposed to just looking at a map. Ms. Stahl suggested that if staff and/or the Commission come to look at the property, she invited them to let herself or another property owner know so they can show them the existing issues.

Mr. Leeper encouraged the Developer to look for ways to ways to make things a little better in the area, rather than just settle for not making them worse. Mr. Arntson asked for clarification of the drainage of Lot 6 to ensure its draining properly.

The matter will be continued at the September 26 Planning and Zoning meeting.
4.) The Commission then considered the Downtown Design Review of 308 Franklin Street. Acting Chair Holst introduced the item and Mr. Sturch provided background information. He explained that the item is in regard to a business that will be relocating to the property at 308 Franklin Street and an addition with a deck is proposed to accommodate ADA accessibility requirements. Staff recommends approval.

Mr. Holst recommended that the roof pitch for the addition match the rest of the building.
Mr. Arntson made a motion to approve with the change to the pitch in the roof. Mr. Leeper seconded the motion. The motion was approved unanimously with 6 ayes (Arntson, Giarusso, Hartley, Leeper, Saul and Wingert), 1 abstention (Holst) and 0 nays.
5.) The next item of business was the Downtown Design Review of 419 Washington Street. Acting Chair Holst introduced the item and Ms. Lehmann provided background information. She explained that the owner of Cottonwood Canyon is proposing to build a new deck on the front of the property to provide more customer seating and visibility for the restaurant. The property was originally built as a single-family home and was eventually converted to a commercial use. The proposal includes moving the free-standing sign to the other side of the walkway to make room for the deck. Ms. Lehmann discussed the requirements for any downtown design changes and noted that all requirements are met. She explained that staff has been working with the petitioner to create a plan that will be appropriate and he is willing to make necessary changes. Staff recommends approval with the following stipulations:

1) The wood deck, existing wood ramp, and stoop structure must be painted or stained with an opaque color that is consistent with the color of the building.
2) Applicant modifies the size of the deck to ensure it is setback a minimum of 3 feet from the public sidewalk and provides a landscape plan including how shrubs for the area between the deck and the sidewalk.
3) Additional details are provided for the construction of the deck. Specifically the type of skirting and balustrade details as described in the staff report
4) The liquor license is amended to include the area of the new deck.

Randolph Brian, Cottonwood Canyon, stated that he is comfortable with any adjustments to the porch and thinks it will enhance the business. He also noted that the neighbors on either side are aware of the project and neither had objections.

Ms. Saul made a motion to approve. Ms. Giarusso seconded the motion. The motion was approved unanimously with 6 ayes (Arntson, Giarusso, Hartley, Leeper, Saul and Wingert), 1 abstention (Holst) and 0 nays.
6.) The next item for consideration by the Commission was the Downtown Design Review of 408 - 412 Main Street. Acting Chair Holst introduced the item and Ms. Lehmann provided background information. She explained that the owners would like to paint a mural over the entire south side of the building. The proposal will not affect the primary façade of the building but it is highly visible and will bring vibrancy to what is currently a blank wall. Ms. Lehmann showed a rendering of the proposed mural, which is the artist's abstract representation of Cedar Falls. Staff feels the mural is appropriate for the location. The applicants have agreed to maintain the mural over time. Staff recommends approval with any recommendations from the Planning and Zoning Commission.

Ms. Wingert asked if this is being approved just because it is on a secondary façade. Ms. Lehmann confirmed that this is one of the main reasons for approval. Mr. Hartley asked why the Commission approves this kind of artwork and who handles and appropriates the art that is currently downtown. Ms. Lehmann noted that the code requires review of Murals on private property by the Planning and Zoning Commission. She added that she believes that the Public Art Commission works with UNI to rotate the sculptures throughout the downtown's public right of way. Carol Lilly, Community Main Street, explained that the difference is that there are no public funds being used for this project and this is also a private property, as such review by the Public Art Committee is not required. Mr. Arntson asked what the rules would be if someone wanted to paint brickwork that should be preserved. Ms. Lehmann clarified that it is highly discouraged in the Code. Mr. Leeper noted that he is uncomfortable with the Commission approving art and feels that a cursory visit with the Public Art Committee would be more appropriate. Ms. Lehmann stated that the Commission isn't really being asked to provide feedback on the art itself, but rather the objective parts of the project, such as location, painting of a wall, colors, appropriateness of the general content/message etc. There was further discussion on whether the Commission should approve art.

Ms. Saul made a motion to approve. Mr. Arntson seconded the motion. The motion was approved with 4 ayes (Arntson, Giarusso, Saul and Wingert), 2 nays (Hartley and Leeper) 1 abstention (Holst) and 0 nays.
7.) The Commission then received updates regarding future community visioning process and zoning code update for downtown. Ms. Howard reviewed presentations and discussions that have taken place regarding the Comprehensive Plan and Zoning Ordinance updates and

## Item 2.

explained that the City Council directed staff to move forward with an RFP for a consultant experienced in visioning and the development of form-based zoning codes. Ms. Howard explained the next steps going forward with the process of working with a consultant. She discussed recent coordination with an upcoming parking study that will be used to help with updating the code. She explained the public outreach and visioning portion of the design process as well as the development of the code. Mr. Leeper noted that it takes a while to get through this process and we're doing a small chunk of the City and wondered if there are some broad planning moves for the entire city that might be considered that aren't as detailed as this will be a long process. Ms. Howard said that staff is trying to balance schedules and the rest of the work being done within the City while being sure to do things correctly.
8.) As there were no further comments, Mr. Leeper made a motion to adjourn. Mr. Wingert seconded the motion. The motion was approved unanimously with 6 ayes (Arntson, Giarusso, Hartley, Leeper, Saul and Wingert), 1 abstention (Holst) and 0 nays.

The meeting adjourned at 6:50 p.m.

Respectfully submitted,


Community Services Manager


DEPARTMENT OF COMMUNITY DEVELOPMENT

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MEMORANDUM
Planning \& Community Services Division

TO: Planning \& Zoning Commission
FROM: Shane Graham, Planner II
DATE: October 4, 2018
SUBJECT: Fleet Farm Retail and Convenience Store Site Plan Review

REQUEST: Site plan approval for a new Fleet Farm retail store and convenience store.
PETITIONER: Midland Atlantic Development Company, LLC, Buyer; Bayer Becker, Engineer
LOCATION: Southwest corner of Highway 58 and West Ridgeway Avenue

## PROPOSAL

The applicant, Midland Atlantic Development Company, proposes to construct a new 257,000 square foot Fleet Farm retail store with yard area, along with a new Fleet Farm convenience store on approximately 49 acres of land located at the southwest corner of Highway 58 and W
Ridgeway Avenue. Three future retail buildings totaling 55,000 square feet are also shown on the site plan just to the north of the retail building, but are not part of this site plan review request. Also, the overall development plan shows additional buildable area along the east side of the property, which is not part of this site plan review and will need to come back before the Planning \& Zoning Commission in the future for approval.


## BACKGROUND

The applicant has an agreement to purchase the property, and is currently requesting to rezone it from Agricultural to Commercial in order to develop it into the intended commercial use. This report will focus on the Fleet Farm retail store and convenience store only, along with the site development elements of this project.

ANALYSIS
Please note that for purposes of this analysis, staff is assuming that the property is zoned HWY1, Highway Commercial District. The City Council approved the first reading of a conditional rezoning of this property from A-1 Agricultural to HWY-1 Highway Commercial on October 1, 2018, and the third and final reading of the rezoning ordinance is scheduled to be presented to City Council on November 5, 2018. As you may recall, the rezoning is subject to certain conditions that are included in a conditional zoning agreement, as summarized below:

1. All street, intersection, traffic control improvements and any additional right-of-way necessary to provide for safe and efficient traffic control and circulation to serve the long term needs of the subject development at full build out of the Property without causing undue traffic circulation and congestion problems along the adjacent public street corridors must be dedicated, constructed, and accepted prior to issuance of an occupancy permit for any portion of development on the Property. Further, these improvements shall be specified and delineated in a developmental agreement between the Applicant or the then-owner of the property and the City prior to approval of the first site plan for development of the property;
2. The area shown as "Future R/W (right-of-way)" on the concept site plan shall remain as open space and shall not be developed with any structures, fences, buildings, hard surfacing, driveways or sidewalks;
3. If and when the property to the west ever redevelops with commercial uses, a 20-foot wide cross-access drive shall be constructed by the property owner at their expense within a 30 -foot wide cross-access easement that will be established at the time of site plan approval. The exact location of the easement will be determined with the site plan;
4. A 5 -foot wide sidewalk shall be installed along the entire frontage of the property along W. Ridgeway Avenue. The City will work with the developer to determine the best location for the easternmost sidewalk segment to avoid the wetland and provide for safe pedestrian access to the corner of Ridgeway and Hwy 58. The installation of the sidewalk shall be completed prior to issuance of an occupancy permit for the first building constructed on the development site;
5. Sidewalks shall be installed throughout the interior of the development site to provide a continuous sidewalk network between all the commercial buildings on the site. A sidewalk network plan shall be required at the time of site plan review.

The HWY-1 district is intended to promote general service commercial uses intended to serve a broader market area (i.e. city-wide or regional customer base). The property is also located within the Highway 20 Overlay Zoning District, which provides enhanced development guidelines for commercial uses located within this corridor. The ordinance requires detailed site plan review prior to approval in order to ensure that the development site satisfies a number of basic aesthetic standards. Attention to details such as parking, open green space, landscaping, signage, building design and other similar factors help to ensure orderly development in the entire area. Following is a review of the zoning ordinance requirements:

1) Use: A big box retail store and convenience store can have a regional customer base, thus fitting within the permitted uses of the HWY-1 District. Such a use is also allowed within the Highway 20 Overlay Zoning District. Use is allowed.
2) Setbacks: 20-ft. setbacks are required along the edge of the district and along any internal streets/principal access ways. These areas must be landscaped. Open space and landscaping is shown on the plan within these areas. Both the retail store and convenience store meet the setback requirements. Building setbacks are satisfied.

## 3) Parking/Access:

a. Parking - For retail stores over 2,000 square feet in size, it is required to provide 4.5 parking spaces for each 1,000 square feet of gross floor area. A convenience store is required to provide 1 parking space for every 100 square feet of retail floor space. Based on the gross floor area, the big box retail store will be required to provide 750 parking spaces, and the convenience store will be required to provide 24 parking spaces, for a total of 774 spaces. 1,096 parking spaces are shown on the submitted site plan, which far exceeds the requirements for the buildings included with this site plan review. The additional parking spaces are being constructed in anticipation of providing for the additional parking needs of the future retail buildings and other buildable areas on the site, which are not being reviewed with this application.
b. Cross Access - One of the conditions in the conditional zoning agreement is that when the property to the west ever redevelops with commercial uses, a crossaccess drive shall be constructed by the property owner at their expense within a 30foot wide cross-access easement. The exact location of the easement is to be shown on the site plan. The site plan does show a 30 -foot wide cross access easement located just to the north of the retail building, and this easement will need to be recorded at the time of site plan approval. This drive will not need to be constructed, unless and until the property to the west redevelops.
c. Reserved area for future improvements to the interchange of Highway 20 and Highway 58 - Another condition in the conditional zoning agreement is that the site plan reserve an area for future right-of-way and that the area shall remain as open space and shall not be developed with any structures, fences, buildings, hard surfacing, driveways or sidewalks. This reserved open space is shown on the site plan, so this condition has been addressed. When the land is platted this area should be included as an outlot with the purpose clearly stated. If in the future the IDOT determines that this land is not needed for improvements to the highway interchange, development of the land for commercial purposes could be considered under the zoning standards in place at that time.
d. Street Access - The property currently has one farm access driveway off W Ridgeway Avenue. Although this property has frontage along both Highway 58 and US Highway 20, no access will be allowed from those frontages. The site plan shows two new access points to the site: one across from Nordic Drive, and one across from a shared drive that serves two residential dwellings along the north side of W Ridgeway Avenue.

In order to determine the improvements to the public roadways necessary to support the development of this property, the City required the applicant to conduct a traffic study. The traffic study was originally submitted to the City on July 23, 2018, and a review of the study was completed by City staff and by City-hired peer review. It was determined that certain corrections needed to be made to the study to adequately address future traffic impacts, including assessment of a number of alternative options for access at the proposed main entrance and at Nordic Drive.

On October 1, 2018, the applicant submitted a revised traffic study to the City, which is currently under review. Although the revised traffic study has not yet been fully reviewed by the City, the developer has indicated that their recommendation, based on the traffic study, is for the main entrance drive to be a controlled intersection (roundabout), and that the proposed access drive across from Nordic Drive be limited to right in, right-out movements only. Therefore, the site plan submitted for review illustrates the concept of a 2 -lane roundabout at their main entrance, which is situated at the location of their current farm access and across from two residential driveways on the north side of W Ridgeway Avenue, and a right-in, right-out access across from Nordic Drive. The concept of a right-in, right-out only access to the site across from Nordic Drive includes the closing of the median at the intersection of Nordic Drive and W Ridgeway Avenue to prevent illegal and unsafe left turns into the site. A consequence of closure of the median would be that drivers travelling southbound on Nordic Drive would not be able to turn left (east) onto W Ridgeway Avenue, and instead would have to turn right (west) onto W Ridgeway Avenue. If a driver did wish to get to Highway 58, the vehicle could go through the proposed roundabout which would be located 500 feet to the west, and make a 180 -degree turn and proceed east on W Ridgeway Avenue to Highway 58.

Because these or alternative roadway improvements will affect traffic movements to and from W Ridgeway Avenue from Nordic Drive, staff is scheduling a public meeting with other property owners and businesses that may be affected by future changes to the roadway. The applicant has agreed to attend the meeting and present the traffic study findings and their recommended roadway improvements. City staff is working with a third party to conduct a peer review of the revised traffic study. This review will be completed by early next week. After the City and peer reviewer has completed the review, City staff will make a recommendation on the traffic improvements that are determined to best serve all users in the corridor while maintaining safe and efficient traffic movement into the future. This recommendation may or may not be consistent with the concept currently shown on the submitted site plan.

- The submitted plan for parking satisfies City requirements.
- The plan for cross access meets the requirements of the conditional zoning agreement, provided an easement is recorded.
- The submitted site plan does not show any development in the area required to be reserved for potential future IDOT improvements at the interchange of Hwy 20 and Hwy 58, so meets the requirement of the conditional zoning agreement.
- The plan for street access and associated roadway improvements is still under review.

4) Open Green Space: This property is located within the Highway 20 Commercial Corridor Overlay Zoning District. This overlay district requires that open green space/landscape area be provided at the rate of $15 \%$ of the development site. Following is a summary from the landscape plan that details how this provision is met.

| Development Site | 36.56 Acres |  |
| :--- | :---: | ---: |
| Required Open/Green Space | 5.48 Acres | 15\% |
| Provided Open/Green Space | $\mathbf{1 2 . 1 1}$ Acres | $\mathbf{3 3 \%}$ |

Landscaping is shown throughout the site, both around the buildings as well as within the parking lot and along the street frontages. A protected wetland is located along the frontage of the property along both Ridgeway and along Highway 58. Some disturbance of the wetland area will be necessary to provide access to the site and these wetland impacts will have to be mitigated. The applicant has indicated that they plan to purchase wetland bank credits to satisfy U.S. Army Corp of Engineers mitigation requirements. The applicant has received approval of a permit based on their mitigation plan. However, if additional disturbance of the wetlands is necessary due to recommended roadway improvements, it may be necessary to seek additional federal approvals.

The open green space exceeds the minimum requirement and is well distributed.
5) Landscaping: The Highway 20 Commercial Corridor Overlay Zoning District requires landscaping at the rate of 0.02 points per sq. ft. of total development site area. Following are the requirements for the retail and convenience store sites and what is proposed.

| Description | Required | Proposed |
| :---: | :---: | :---: |
| Development Lot | 27,076 pts. | 38,295 pts. |
| Parking lot trees 1,096/15 = 73 trees @ 80 pts. | 5,840 pts. | 15,580 pts. |
| Street Tree Planting <br> (. 75 points per linear foot) | 2,784 pts. | 2,880 pts. |

As detailed in the table, trees are required in the vehicular use area at the rate of one tree per 15 parking spaces. With a total buildout of 1,096 parking spaces, 73 trees would be required. The landscape plan shows a total of 79 trees, which would meet the requirement.

In addition to parking lot trees, there are trees and shrubs located along the perimeter of the parking areas, as well as trees located along the street frontages. In total, there will be 319 overstory trees, 27 understory trees, 195 evergreen trees and over 500 shrubs planted on the site. Landscaping requirements are met.
6) Sidewalks/Recreational Accommodations: Whenever a new development is proposed, City Code requires the developer to install a sidewalk along the entire street frontage of
the property. On this particular property, there is no sidewalk currently located along W Ridgeway Avenue. However, there is a recreational trail located along the north side of W. Ridgeway Avenue at Nordic Drive, and along the south side of W Ridgeway Avenue east of Highway 58. Adding a sidewalk section in front of this development will connect the two trail networks, which in turn benefits the community as a whole. There may be some challenges to installing the sidewalk along the entire street frontage, as there is a drainage way located near W Ridgeway Avenue. It is the responsibility of the developer to determine how best to provide a sidewalk in this location. City staff will work with the developer if an alternative location or design is necessary due to site and roadway conditions. It should be noted that the recommended roadway improvements to W . Ridgeway Avenue may affect the location of the sidewalk.

Also, since this large property includes a number of separate buildings sites with the drives providing circulation similar to a street network, one of the conditions of the rezoning is that sidewalks be installed throughout the interior of the development site to provide a continuous sidewalk network between all the commercial buildings on the site. Five-foot wide sidewalks are shown throughout the interior of the site to provide pedestrian connections to each of the buildings and future outlots on the site. This will allow customers to park once and walk safely between multiple businesses during their visit. One minor correction to the sidewalk network plan is noted: a future sidewalk segment should be illustrated along the areas labeled future outlots on the site plan. These segments will not have to be constructed until those areas are developed.

Interior sidewalk plan is acceptable, provided a future sidewalk section is illustrated along the future outlots. The final location of the required sidewalk along W . Ridgeway Avenue is still being determined and will be based on the recommended improvements to W. Ridgeway Avenue.
7) Building Design: The HWY-1, Highway Commercial District, states that all structures established within the district shall be reviewed for architectural compatibility with surrounding structures. Below is a review on the elements that are to be addressed.

Proportion: The relationship between the width and height of the front elevations of adjacent buildings shall be considered in the construction or alteration of a building; the relationship of width to height of windows and doors of adjacent buildings shall be considered in the construction or alteration of a building.

The scale and proportion of the new retail building and convenience store will be similar to the existing businesses located nearby. There are several hotels nearby with heights varying between two and four stories, with several commercial and industrial buildings in the area that are one story in height. Both the retail store and convenience store will be one story in height. The size of the retail store ( 185,000 square feet) is larger than most buildings in this area, however because it is on a very large site, the size would not appear to be out of character for the area.

Roof shape, pitch, and direction: The similarity or compatibility of the shape, pitch, and direction of roofs in the immediate area shall be considered in the

The design of the both the retail store and convenience store incorporates a flat roof with a parapet wall. The Kwik Star convenience store directly to the north utilizes a similar roof design, while the nearby hotels use a gable roof design. Nearby industrial buildings located within the industrial park utilize a similar flat roof design as well, so this roof design will not be out of character with the area.

Pattern: Alternating solids and openings (wall to windows and doors) in the front facade and sides and rear of a building create a rhythm observable to viewers. This pattern of solids and openings shall be considered in the construction or alteration of a building.

The retail building was designed with textured precast concrete panels in two different tones of gray and with different patterns etched into them to provide some visual interest to the long building walls. This pattern carries through the entirety of the building. The convenience store was similarly designed with the textured concrete precast panels in the two different tones of gray, in order to give it a similar look to the large retail building. The primary façade of the big box store has alternating pattern of window and main entrance features that provide views and openings into the building. These are alternated with the precast concrete panels, separate modules of phenolic panels, some with an aged cedar wood appearance and some in Fleet Farm Orange. Decorative metal awnings also help to visually break up the long facades. All of these elements provide a visually interesting rhythm to the primary façade. The rear and sides of the store will feature mainly the precast concrete panels, along with several overhead doors and service doors. The south side of the building will also feature an auto repair area. There are no façade variations along the rear and sides of the building, however these areas will not be highly visible to neighboring properties to the west because of a large landscaped berm that will be located along the western property line, or the public right-of-way to the south due to the location of future retail buildings and the large amount of trees that will remain along the drainage way at the north end of the property.

The primary façade of the convenience store has an alternating pattern of windows, two types of textured precast concrete panels, and Fleet Farm Orange phenolic panels. These features provide a visually pleasing main entrance into the building. The sides and rear of the building do not have windows due to the location of the attached car wash, however these facades are patterned with the two types of textured precast concrete panels, in addition to the Fleet Farm Orange phenolic paneling which rises above the main roof line to give the building added visual depth.

Materials and texture: The similarity or compatibility of existing materials and textures on the exterior walls and roofs of buildings in the immediate area shall be considered in the construction or alteration of a building. A building or alteration shall be considered compatible if the materials and texture used are appropriate in the context of other buildings in the immediate area.


The front of the retail store will feature the two tones of textured concrete precast panels, as well as a white metal perforated panel with the company's name and logo located above the main entrance. Phenolic panels in an aged cedar wood color will be installed just to the south of the main entrance to give it a more modern look and feel. Also, at the northeast corner of the building will be Fleet Farm Orange phenolic paneling that that wraps around the corner of the building and will feature the company logo. The retail store will also feature a yard area at the south end of the building, which will be surrounding by a 16 ' tall wood fence at the south side and an 8' tall metal/slatted chain link fence on the east and west sides. Staff notes that the wood fence should be stained or painted to provide a more finished look visible from Highway 20 and to prevent deterioration. The south side of the store will feature an auto repair area, so several large overhead doors will be located on this side.


Yard Fence at Barn - 16'

The convenience store will also feature the two tones of textured precast concrete panels, as well as the Fleet Farm Orange phenolic paneling located along portions of all four sides of the building.

Color: The similarity or compatibility of existing colors of exterior walls and roofs of buildings in the area shall be considered in the construction or alteration of a building.

Many of the existing buildings in this area utilize a neutral color exterior, which include brown, tan, and cream. Some buildings also utilize red or gray tones as well. The retail store and convenience store will include two shades of gray in the textured precast concrete panels, with areas of the signature Fleet Farm Orange highlighted on several areas of the buildings. Staff feels that the amount of the orange that is incorporated into the two buildings does not take away from the overall look of the development and provides additional visual interest to the facades.

Architectural features: Architectural features, including but not limited to, cornices, entablatures, doors, windows, shutters, and fanlights, prevailing in the immediate area, shall be considered in the construction or alteration of a building. It is not intended that the details of existing buildings be duplicated precisely, but those features should be regarded as suggestive of the extent, nature, and scale of details that would be appropriate on new buildings or alterations.

Architectural features of the retail store include two large curtain walls of windows on the front of the building, along with the raised perforated metal panel located above the main entrance. The convenience store will have typical windows located on it, and also incorporates the Fleet Farm Orange phenolic panels on it to give it contrast from the gray textured concrete precast panels.

## Overall, the design of the retail store and convenience store is architecturally compatible with other buildings in the surrounding area.

8) Trash Dumpster Site: A trash compactor will be located within the Fleet Farm building near the truck loading docks at the southwest corner of the building. Also, a trash dumpster enclosure is located at the north end of the convenience store parking lot. This enclosure will be constructed with textured precast concrete, with a color matching gate. The color of the enclosure will match the color of the convenience store building. Dumpster enclosure plan is acceptable.
9) Lighting Plan: The HWY-1 District regulations do not have specific lighting design guidelines. The site plan shows the location of light poles and all wall lights throughout the site. The parking lot lights will be mounted atop 38' tall light poles and will include a single head fixture. These fixtures will be housed in a die-cast aluminum housing with


LED lights. Also, wall mounted lights will be located on the walls of the building in various locations, and surface mounted downlights will be located under the petroleum canopy. Lighting plan is acceptable.
10) Signage: Three (3) monument signs are illustrated on the site plan in different locations on the property. The main sign (as shown to the right), located near the south end of the property along U.S. Highway 20, will be 25 feet in height and 200 square feet in area. The sign will sit on a stone veneer base that matches the color of the building. Below the sign lettering will be an LED reader board for messaging.

Two smaller 15' tall signs will be located near the corner of Highway 58 and W Ridgeway Avenue and near the eastern entrance to the property along W Ridgeway Avenue. One of the signs will be 150 square feet in area and
 the other will be 118.6 square feet in area. These signs will also have a stone veneer base that matches the color of the building, but will not have an LED reader board.

It should be noted that the property is located within the Highway 20 Commercial Corridor Overlay Zoning District. The signage requirements in this district state that one freestanding sign may be allowed that does not exceed 25 feet in height and 200 square feet in area. The main monument sign would meet those requirements. The ordinance goes on to state that smaller monument signs, measuring no more than 15 feet in height and 150 square feet in area, are permitted, with a maximum of two such signs per parcel. The two additional signs on the property would meet these requirements as well.

The proposed wall signs appear to be well within the District limitations of no larger than $20 \%$ of the wall area to which the wall sign is attached. However, this will be reviewed in detail at the time a sign permit is requested. Signage plan is acceptable, subject to detailed review with a sign permit.
11) Storm Water Management: A total of three (3) storm water detention basins will be located on the property to collect the storm water runoff from the site. Basin \#1 as shown on the plan will be located within the main parking lot area, east of the future retail buildings. This basin will collect water from a majority of the development site. The water from this basin will be released at a controlled rate via a pipe into Basin \#2. Basin \#2 as shown on the plan will be located just west of the convenience store and north of the main parking lot. This basin will collect water from the convenience store, and also the water from Basin \#1. The water will then be released at a controlled rate into the drainage way and wetland located along the north side of the property along W Ridgeway Avenue. Basin \#3 as shown on the plan will collect water from the remaining southern half of the development. The water will then be released at a controlled rate into the drainage ditch to the east along Highway 58. Stormwater Management Plan is currently being reviewed by the Engineering Department.

## TECHNICAL COMMENTS

The rezoning of this property from A-1 to HWY-1 was recommended for approval by the Planning \& Zoning Commission on July 25, 2018. As part of that recommendation of approval, five conditions were attached in order to keep the rezoning process moving forward, as the City had not seen a traffic impact study at that time. It should be noted that a traffic impact study was submitted to the City on Monday, October 1, 2018, and is currently under review by City staff and a third party. The first condition was that all street, intersection, and traffic control improvements necessary to provide for safe and efficient traffic control and circulation to serve the long term needs of the subject development at full build out without causing undue traffic circulation and congestion problems be designed and agreed upon prior to approval of the rezoning at City Council and that said improvements must be constructed and accepted by the City prior to issuance of an occupancy permit for any portion of the development. To date, this has not been designed and agreed upon because the City is still reviewing the traffic study. The design of the improvements should be agreed upon by the City and developer by the time of the next Planning \& Zoning Commission meeting on October 24th. The second condition was area shown as "Future R/W (right-of-way)" on the concept site plan shall remain as open space and shall not be developed with any structures, fences, buildings, hard surfacing, driveways or sidewalks. This open space is shown on the site plan, so this condition has been addressed. The third condition was that and when the property to the west ever redevelops with commercial uses, a cross-access drive shall be constructed by the property owner at their expense within a 30 -foot wide cross-access easement that will be established at the time of site plan approval. The exact location of the easement will be determined with the site plan. The site plan does show a 30 -foot wide cross access easement just to the north of the retail building, however, this easement will need to be recorded. This drive would not need to be constructed until the property to the west redevelops. The fourth condition was that 5 -foot wide sidewalk shall be installed along the entire frontage of the property along W. Ridgeway Avenue. The City will work with the developer to determine the best location for the easternmost sidewalk segment to avoid the wetland and provide for safe pedestrian access to the corner of Ridgeway and Hwy 58. The site plan shows a 5' wide sidewalk along the W Ridgeway Avenue street frontage; however staff is concerned with how close the sidewalk gets to W Ridgeway Avenue as you go east of Drive \#2 across from Nordic Drive. Additional analysis may be needed to see if there is a more suitable, safer location for this sidewalk. The fifth condition was that sidewalks shall be installed throughout the interior of the development site to provide a continuous sidewalk network between all the commercial buildings on the site. The site plan shows interior sidewalks that lead from the main retail building to the future retail buildings to the north and east to the convenience store. As noted in the staff report, a future sidewalk segment should be shown across the frontage of the area labelled as future outlots. This will not have to be installed until that are is developed, but should be shown on the plan.

Several technical comments were made by Cedar Falls Utilities staff regarding utility locations, and the Engineering Division has made technical comments regarding sanitary sewer and storm water facilities. These comments were sent to the developer to be addressed.

Water, electric, gas, and communications utility services are available to the site in accordance with the service policies of Cedar Falls Utilities. The property owner/contractor is responsible to extend all utility services to the building. These utility extensions will be reviewed by CFU personnel as part of the building plan review.

## Item 4.A.

## STAFF RECOMMENDATION

The introduction of this site plan is for discussion and public comment purposes only. The Community Development Department has reviewed the plan and provides the following comments:

1) Review of traffic study to determine the public roadway improvements necessary for this development.
2) Review of the location of the sidewalk along W Ridgeway Avenue.
3) Corrections needed based on technical comments.
4) Any comments or direction specified by the Planning \& Zoning Commission.

Subject to the issues and comments noted above being addressed, staff anticipates that this will be referred to the Planning and Zoning Commission for a vote on October 24, 2018.

## PLANNING \& ZONING COMMISSION

Discussion
10/10/2018
Vote
10/24/2018

## Cedar Falls Planning \& Zoning Commission October 10, 2018
















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| contributing drainage area is 10 acres or less．The outlet is an earthenembankment with a simple stone spillway underlain with woven geotextile．Asediment basin is appropriate for drainage areas larger than 10 acres．The |  |
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