

AGENDA CITY OF CEDAR FALLS, IOWA PLANNING AND ZONING COMMISSION WEDNESDAY, OCTOBER 10, 2018 5:30 PM AT CITY HALL - COUNCIL CHAMBERS

- Call to Order and Roll Call
- 2. Approval of Minutes
- 3. Public Comments
- 4. New Business

A. HWY-1 District Site Plan Review – Fleet Farm Retail and Convenience Store

Location: SW Corner of Highway 58 and W. Ridgeway Avenue

Applicant: Midland Atlantic Development Company, L.L.C.

Previous Discussion: None

Staff Recommendation: Introduction and Discussion

P&Z Action Needed: Gather comments and continue discussion at the October 24,

2018 P&Z meeting.

5. Old Business

A. Park Ridge Estates Preliminary Plat (Discussion Deferred to October 24)

Location: 20.8 acre property at the north end of Lakeshore Drive Applicant: Larry Hill, owner; Wingert Development, CGA, Inc. Engineer

Previous Discussion: September 12, 2018

Staff Recommendation: Discussion Deferred to October 24 meeting

P&Z Action Needed: None

- 6. Commission Updates
- 7. Adjournment

Reminders:

- October 24th and November 14th Planning & Zoning Commission Meeting
- October 15th and November 5th City Council Meeting

Cedar Falls Planning and Zoning Commission Regular Meeting September 12, 2018 City Hall Council Chambers 220 Clay Street, Cedar Falls, Iowa

MINUTES

The Cedar Falls Planning and Zoning Commission met in regular session on Wednesday, September 12, 2018 at 5:30 p.m. in the City Hall Council Chambers, 220 Clay Street, Cedar Falls, Iowa. The following Commission members were present: Arntson, Giarusso, Hartley, Holst, Leeper, Saul and Wingert. Adkins and Oberle were absent. Karen Howard, Community Services Manager, David Sturch, Planner III and Iris Lehmann, Planner I, were also present.

- 1.) Acting Chair Holst noted the Minutes from the August 22, 2018 regular meeting are presented. Ms. Giarusso made a motion to approve the Minutes as presented. Mr. Hartley seconded the motion. The motion was approved unanimously with 7 ayes (Arntson, Giarusso, Hartley, Holst, Leeper, Saul and Wingert) and 0 nays.
- 2.) The first item of business was a continuation of public hearing regarding a rezoning request at the end of Lakeshore Drive from A-1, Agricultural to R-1, Residential. Acting Chair Holst introduced the item and Mr. Wingert noted that he will be abstaining from the item. Mr. Sturch provided background information, explaining that the item was introduced at the last meeting. It is a 20 acre parcel at the end of Lakeshore Drive that is proposed for residential use. He noted that utilities meet requirements, as well as the land use map. Staff recommends approval with conformance to all city staff recommendations and technical comments as well as any comments or direction from the Commission.

Tamie Stahl, 1009 Lakeshore Drive, stated that she was told when they moved into their home they were told this area would never be rezoned as there were too many issues and asked how it could be considered at this time.

Mr. Adam Daters, Clapsaddle Garber Associates, stated that he believes all criteria for rezoning have been met.

Ms. Saul noted that she feels that it is straightforward and consistent with the land use map. Mr. Holst agreed.

Mr. Leeper made a motion to approve. Mr. Arntson seconded the motion. The motion was approved unanimously with 5 ayes (Arntson, Giarusso, Hartley, Holst, Leeper and Saul), 2 abstentions (Holst and Wingert) and 0 nays.

3.) The next item for consideration by the Commission was the Park Ridge Estates Preliminary Plat. Acting Chair Holst introduced the item and Mr. Sturch provided background information. He explained that approximately 9 acres of the parcel is in the floodplain, it contains significant tree stands, steep slopes and meandering streams from the west and south to the Cedar River. He noted that the goal is to maintain those things, and that there is a required review for grading, storm water detention and runoff. The design is to create a controlled runoff for that area. Mr. Sturch displayed the proposed plat and identified the lots and easements, as well as detention basins. He explained the proposed plans for the runoff to divert into a ravine. Staff would like to discuss the plat at this time and gather any comments for continued discussion at the next Planning and Zoning meeting.

Item 2.

Mr. Adam Daters, Clapsaddle Garber Associates, came forward to clarify that they are in agreement with the additional requirements and their intent is to leave the area as natural as possible. Mr. Holst asked if there any consideration of a future extension of Lakeshore Drive. Mr. Daters noted that there wasn't any discussion on that. Mr. Leeper stated his concern for the storm water and asked if it might be better to release it into the stream versus a controlled release. Mr. Arntson asked about the elevation and drainage, and where the water will go. Mr. Daters provided answers to each inquiry.

Mr. Leeper asked if Mr. Daters is comfortable stating that this will not make things worse. Mr. Daters explained stated that they don't affect it currently and that it will not be worse.

Tamie Stahl, 1009 Lakeshore Drive, noted concerns about the development and said that the Lakeshore Development has not been treated well. She reiterated concerns with storm water management and showed the debris that come through the pond whenever there is a greater amount of water flow. She also passed out pictures of the issues in the area when it rains.

Lisa Sage, owner of the lot on corner of Lakeshore and Lilliput, asked for clarification about runoff from the south. Mr. Daters explained that they will be trying to push the water drainage further to the north.

Ms. Saul recommended that staff walk the property to get a better view as opposed to just looking at a map. Ms. Stahl suggested that if staff and/or the Commission come to look at the property, she invited them to let herself or another property owner know so they can show them the existing issues.

Mr. Leeper encouraged the Developer to look for ways to ways to make things a little better in the area, rather than just settle for not making them worse. Mr. Arntson asked for clarification of the drainage of Lot 6 to ensure its draining properly.

The matter will be continued at the September 26 Planning and Zoning meeting.

- 4.) The Commission then considered the Downtown Design Review of 308 Franklin Street. Acting Chair Holst introduced the item and Mr. Sturch provided background information. He explained that the item is in regard to a business that will be relocating to the property at 308 Franklin Street and an addition with a deck is proposed to accommodate ADA accessibility requirements. Staff recommends approval.
 - Mr. Holst recommended that the roof pitch for the addition match the rest of the building.
 - Mr. Arntson made a motion to approve with the change to the pitch in the roof. Mr. Leeper seconded the motion. The motion was approved unanimously with 6 ayes (Arntson, Giarusso, Hartley, Leeper, Saul and Wingert), 1 abstention (Holst) and 0 nays.
- 5.) The next item of business was the Downtown Design Review of 419 Washington Street. Acting Chair Holst introduced the item and Ms. Lehmann provided background information. She explained that the owner of Cottonwood Canyon is proposing to build a new deck on the front of the property to provide more customer seating and visibility for the restaurant. The property was originally built as a single-family home and was eventually converted to a commercial use. The proposal includes moving the free-standing sign to the other side of the walkway to make room for the deck. Ms. Lehmann discussed the requirements for any downtown design changes and noted that all requirements are met. She explained that staff has been working with the petitioner to create a plan that will be appropriate and he is willing to make necessary changes. Staff recommends approval with the following stipulations:

- 1) The wood deck, existing wood ramp, and stoop structure must be painted or stained with an opaque color that is consistent with the color of the building.
- 2) Applicant modifies the size of the deck to ensure it is setback a minimum of 3 feet from the public sidewalk and provides a landscape plan including how shrubs for the area between the deck and the sidewalk.
- 3) Additional details are provided for the construction of the deck. Specifically the type of skirting and balustrade details as described in the staff report
- 4) The liquor license is amended to include the area of the new deck.

Randolph Brian, Cottonwood Canyon, stated that he is comfortable with any adjustments to the porch and thinks it will enhance the business. He also noted that the neighbors on either side are aware of the project and neither had objections.

Ms. Saul made a motion to approve. Ms. Giarusso seconded the motion. The motion was approved unanimously with 6 ayes (Arntson, Giarusso, Hartley, Leeper, Saul and Wingert), 1 abstention (Holst) and 0 nays.

6.) The next item for consideration by the Commission was the Downtown Design Review of 408 – 412 Main Street. Acting Chair Holst introduced the item and Ms. Lehmann provided background information. She explained that the owners would like to paint a mural over the entire south side of the building. The proposal will not affect the primary façade of the building but it is highly visible and will bring vibrancy to what is currently a blank wall. Ms. Lehmann showed a rendering of the proposed mural, which is the artist's abstract representation of Cedar Falls. Staff feels the mural is appropriate for the location. The applicants have agreed to maintain the mural over time. Staff recommends approval with any recommendations from the Planning and Zoning Commission.

Ms. Wingert asked if this is being approved just because it is on a secondary façade. Ms. Lehmann confirmed that this is one of the main reasons for approval. Mr. Hartley asked why the Commission approves this kind of artwork and who handles and appropriates the art that is currently downtown. Ms. Lehmann noted that the code requires review of Murals on private property by the Planning and Zoning Commission. She added that she believes that the Public Art Commission works with UNI to rotate the sculptures throughout the downtown's public right of way. Carol Lilly, Community Main Street, explained that the difference is that there are no public funds being used for this project and this is also a private property, as such review by the Public Art Committee is not required. Mr. Arntson asked what the rules would be if someone wanted to paint brickwork that should be preserved. Ms. Lehmann clarified that it is highly discouraged in the Code. Mr. Leeper noted that he is uncomfortable with the Commission approving art and feels that a cursory visit with the Public Art Committee would be more appropriate. Ms. Lehmann stated that the Commission isn't really being asked to provide feedback on the art itself, but rather the objective parts of the project, such as location, painting of a wall, colors, appropriateness of the general content/message etc. There was further discussion on whether the Commission should approve art.

Ms. Saul made a motion to approve. Mr. Arntson seconded the motion. The motion was approved with 4 ayes (Arntson, Giarusso, Saul and Wingert), 2 nays (Hartley and Leeper) 1 abstention (Holst) and 0 nays.

7.) The Commission then received updates regarding future community visioning process and zoning code update for downtown. Ms. Howard reviewed presentations and discussions that have taken place regarding the Comprehensive Plan and Zoning Ordinance updates and

Item 2.

explained that the City Council directed staff to move forward with an RFP for a consultant experienced in visioning and the development of form-based zoning codes. Ms. Howard explained the next steps going forward with the process of working with a consultant. She discussed recent coordination with an upcoming parking study that will be used to help with updating the code. She explained the public outreach and visioning portion of the design process as well as the development of the code. Mr. Leeper noted that it takes a while to get through this process and we're doing a small chunk of the City and wondered if there are some broad planning moves for the entire city that might be considered that aren't as detailed as this will be a long process. Ms. Howard said that staff is trying to balance schedules and the rest of the work being done within the City while being sure to do things correctly.

8.) As there were no further comments, Mr. Leeper made a motion to adjourn. Mr. Wingert seconded the motion. The motion was approved unanimously with 6 ayes (Arntson, Giarusso, Hartley, Leeper, Saul and Wingert), 1 abstention (Holst) and 0 nays.

The meeting adjourned at 6:50 p.m.

Respectfully submitted,

Karen Howard

Community Services Manager

Joanne Goodrich Administrative Clerk

Joanne Goodrick



DEPARTMENT OF COMMUNITY DEVELOPMENT

City of Cedar Falls 220 Clay Street Cedar Falls, Iowa 50613 Phone: 319-273-8600 Fax: 319-273-8610

www.cedarfalls.com

MEMORANDUM

Planning & Community Services Division

TO: Planning & Zoning Commission

FROM: Shane Graham, Planner II

DATE: October 4, 2018

SUBJECT: Fleet Farm Retail and Convenience Store Site Plan Review

REQUEST: Site plan approval for a new Fleet Farm retail store and convenience store.

PETITIONER: Midland Atlantic Development Company, LLC, Buyer; Bayer Becker, Engineer

LOCATION: Southwest corner of Highway 58 and West Ridgeway Avenue

PROPOSAL

The applicant, Midland Atlantic Development Company, proposes to construct a new 257,000 square foot Fleet Farm retail store with yard area, along with a new Fleet Farm convenience store on approximately 49 acres of land located at the southwest corner of Highway 58 and W Ridgeway Avenue. Three future retail buildings totaling 55,000 square feet are also shown on the site plan just to the north of the retail building, but are not part of this site plan review request. Also, the overall development plan shows additional buildable area along the east side of the property, which is not part of this site plan review and will need to come back before the Planning & Zoning Commission in the future for approval.



Proposed Development Site

BACKGROUND

The applicant has an agreement to purchase the property, and is currently requesting to rezone it from Agricultural to Commercial in order to develop it into the intended commercial use. This report will focus on the Fleet Farm retail store and convenience store only, along with the site development elements of this project.

ANALYSIS

Please note that for purposes of this analysis, staff is assuming that the property is zoned HWY-1, Highway Commercial District. The City Council approved the first reading of a conditional rezoning of this property from A-1 Agricultural to HWY-1 Highway Commercial on October 1, 2018, and the third and final reading of the rezoning ordinance is scheduled to be presented to City Council on November 5, 2018. As you may recall, the rezoning is subject to certain conditions that are included in a conditional zoning agreement, as summarized below:

- 1. All street, intersection, traffic control improvements and any additional right-of-way necessary to provide for safe and efficient traffic control and circulation to serve the long term needs of the subject development at full build out of the Property without causing undue traffic circulation and congestion problems along the adjacent public street corridors must be dedicated, constructed, and accepted prior to issuance of an occupancy permit for any portion of development on the Property. Further, these improvements shall be specified and delineated in a developmental agreement between the Applicant or the then-owner of the property and the City prior to approval of the first site plan for development of the property;
- 2. The area shown as "Future R/W (right-of-way)" on the concept site plan shall remain as open space and shall not be developed with any structures, fences, buildings, hard surfacing, driveways or sidewalks;
- 3. If and when the property to the west ever redevelops with commercial uses, a 20-foot wide cross-access drive shall be constructed by the property owner at their expense within a 30-foot wide cross-access easement that will be established at the time of site plan approval. The exact location of the easement will be determined with the site plan;
- 4. A 5-foot wide sidewalk shall be installed along the entire frontage of the property along W. Ridgeway Avenue. The City will work with the developer to determine the best location for the easternmost sidewalk segment to avoid the wetland and provide for safe pedestrian access to the corner of Ridgeway and Hwy 58. The installation of the sidewalk shall be completed prior to issuance of an occupancy permit for the first building constructed on the development site;
- 5. Sidewalks shall be installed throughout the interior of the development site to provide a continuous sidewalk network between all the commercial buildings on the site. A sidewalk network plan shall be required at the time of site plan review.

The HWY-1 district is intended to promote general service commercial uses intended to serve a broader market area (i.e. city-wide or regional customer base). The property is also located within the Highway 20 Overlay Zoning District, which provides enhanced development guidelines for commercial uses located within this corridor. The ordinance requires detailed site plan review prior to approval in order to ensure that the development site satisfies a number of basic aesthetic standards. Attention to details such as parking, open green space, landscaping, signage, building design and other similar factors help to ensure orderly development in the entire area. Following is a review of the zoning ordinance requirements:

- 1) <u>Use:</u> A big box retail store and convenience store can have a regional customer base, thus fitting within the permitted uses of the HWY-1 District. Such a use is also allowed within the Highway 20 Overlay Zoning District. **Use is allowed.**
- 2) <u>Setbacks:</u> 20-ft. setbacks are required along the edge of the district and along any internal streets/principal access ways. These areas must be landscaped. Open space and landscaping is shown on the plan within these areas. Both the retail store and convenience store meet the setback requirements. **Building setbacks are satisfied.**

Parking/Access:

- a. Parking For retail stores over 2,000 square feet in size, it is required to provide 4.5 parking spaces for each 1,000 square feet of gross floor area. A convenience store is required to provide 1 parking space for every 100 square feet of retail floor space. Based on the gross floor area, the big box retail store will be required to provide 750 parking spaces, and the convenience store will be required to provide 24 parking spaces, for a total of 774 spaces. 1,096 parking spaces are shown on the submitted site plan, which far exceeds the requirements for the buildings included with this site plan review. The additional parking spaces are being constructed in anticipation of providing for the additional parking needs of the future retail buildings and other buildable areas on the site, which are not being reviewed with this application.
- b. Cross Access One of the conditions in the conditional zoning agreement is that when the property to the west ever redevelops with commercial uses, a cross-access drive shall be constructed by the property owner at their expense within a 30-foot wide cross-access easement. The exact location of the easement is to be shown on the site plan. The site plan does show a 30-foot wide cross access easement located just to the north of the retail building, and this easement will need to be recorded at the time of site plan approval. This drive will not need to be constructed, unless and until the property to the west redevelops.
- c. Reserved area for future improvements to the interchange of Highway 20 and Highway 58 Another condition in the conditional zoning agreement is that the site plan reserve an area for future right-of-way and that the area shall remain as open space and shall not be developed with any structures, fences, buildings, hard surfacing, driveways or sidewalks. This reserved open space is shown on the site plan, so this condition has been addressed. When the land is platted this area should be included as an outlot with the purpose clearly stated. If in the future the IDOT determines that this land is not needed for improvements to the highway interchange, development of the land for commercial purposes could be considered under the zoning standards in place at that time.
- d. Street Access The property currently has one farm access driveway off W Ridgeway Avenue. Although this property has frontage along both Highway 58 and US Highway 20, no access will be allowed from those frontages. The site plan shows two new access points to the site: one across from Nordic Drive, and one across from a shared drive that serves two residential dwellings along the north side of W Ridgeway Avenue.

In order to determine the improvements to the public roadways necessary to support the development of this property, the City required the applicant to conduct a traffic study. The traffic study was originally submitted to the City on July 23, 2018, and a review of the study was completed by City staff and by City-hired peer review. It was determined that certain corrections needed to be made to the study to adequately address future traffic impacts, including assessment of a number of alternative options for access at the proposed main entrance and at Nordic Drive.

On October 1, 2018, the applicant submitted a revised traffic study to the City, which is currently under review. Although the revised traffic study has not yet been fully reviewed by the City, the developer has indicated that their recommendation, based on the traffic study, is for the main entrance drive to be a controlled intersection (roundabout), and that the proposed access drive across from Nordic Drive be limited to right in, right-out movements only. Therefore, the site plan submitted for review illustrates the concept of a 2-lane roundabout at their main entrance, which is situated at the location of their current farm access and across from two residential driveways on the north side of W Ridgeway Avenue, and a right-in, right-out access across from Nordic Drive. The concept of a right-in, right-out only access to the site across from Nordic Drive includes the closing of the median at the intersection of Nordic Drive and W Ridgeway Avenue to prevent illegal and unsafe left turns into the site. A consequence of closure of the median would be that drivers travelling southbound on Nordic Drive would not be able to turn left (east) onto W Ridgeway Avenue, and instead would have to turn right (west) onto W Ridgeway Avenue. If a driver did wish to get to Highway 58, the vehicle could go through the proposed roundabout which would be located 500 feet to the west, and make a 180-degree turn and proceed east on W Ridgeway Avenue to Highway 58.

Because these or alternative roadway improvements will affect traffic movements to and from W Ridgeway Avenue from Nordic Drive, staff is scheduling a public meeting with other property owners and businesses that may be affected by future changes to the roadway. The applicant has agreed to attend the meeting and present the traffic study findings and their recommended roadway improvements. City staff is working with a third party to conduct a peer review of the revised traffic study. This review will be completed by early next week. After the City and peer reviewer has completed the review, City staff will make a recommendation on the traffic improvements that are determined to best serve all users in the corridor while maintaining safe and efficient traffic movement into the future. This recommendation may or may not be consistent with the concept currently shown on the submitted site plan.

- The submitted plan for parking satisfies City requirements.
- The plan for cross access meets the requirements of the conditional zoning agreement, provided an easement is recorded.
- The submitted site plan does not show any development in the area required to be reserved for potential future IDOT improvements at the interchange of Hwy 20 and Hwy 58, so meets the requirement of the conditional zoning agreement.

- The plan for street access and associated roadway improvements is still under review.
- 4) Open Green Space: This property is located within the Highway 20 Commercial Corridor Overlay Zoning District. This overlay district requires that open green space/landscape area be provided at the rate of 15% of the development site. Following is a summary from the landscape plan that details how this provision is met.

Development Site	36.56 Acres	_
Required Open/Green Space	5.48 Acres	15%
Provided Open/Green Space	12.11 Acres	33%

Landscaping is shown throughout the site, both around the buildings as well as within the parking lot and along the street frontages. A protected wetland is located along the frontage of the property along both Ridgeway and along Highway 58. Some disturbance of the wetland area will be necessary to provide access to the site and these wetland impacts will have to be mitigated. The applicant has indicated that they plan to purchase wetland bank credits to satisfy U.S. Army Corp of Engineers mitigation requirements. The applicant has received approval of a permit based on their mitigation plan. However, if additional disturbance of the wetlands is necessary due to recommended roadway improvements, it may be necessary to seek additional federal approvals.

The open green space exceeds the minimum requirement and is well distributed.

5) <u>Landscaping:</u> The Highway 20 Commercial Corridor Overlay Zoning District requires landscaping at the rate of 0.02 points per sq. ft. of total development site area. Following are the requirements for the retail and convenience store sites and what is proposed.

Description	Required	Proposed
Development Lot 1,592,554 * .02	27,076 pts.	38,295 pts.
Parking lot trees 1,096/15 = 73 trees @ 80 pts.	5,840 pts.	15,580 pts.
Street Tree Planting (.75 points per linear foot)	2,784 pts.	2,880 pts.
	35,700 pts	56,755 pts

As detailed in the table, trees are required in the vehicular use area at the rate of one tree per 15 parking spaces. With a total buildout of 1,096 parking spaces, 73 trees would be required. The landscape plan shows a total of 79 trees, which would meet the requirement.

In addition to parking lot trees, there are trees and shrubs located along the perimeter of the parking areas, as well as trees located along the street frontages. In total, there will be 319 overstory trees, 27 understory trees, 195 evergreen trees and over 500 shrubs planted on the site. **Landscaping requirements are met.**

6) <u>Sidewalks/Recreational Accommodations:</u> Whenever a new development is proposed, City Code requires the developer to install a sidewalk along the entire street frontage of

the property. On this particular property, there is no sidewalk currently located along W Ridgeway Avenue. However, there is a recreational trail located along the north side of W. Ridgeway Avenue at Nordic Drive, and along the south side of W Ridgeway Avenue east of Highway 58. Adding a sidewalk section in front of this development will connect the two trail networks, which in turn benefits the community as a whole. There may be some challenges to installing the sidewalk along the entire street frontage, as there is a drainage way located near W Ridgeway Avenue. It is the responsibility of the developer to determine how best to provide a sidewalk in this location. City staff will work with the developer if an alternative location or design is necessary due to site and roadway conditions. It should be noted that the recommended roadway improvements to W. Ridgeway Avenue may affect the location of the sidewalk.

Also, since this large property includes a number of separate buildings sites with the drives providing circulation similar to a street network, one of the conditions of the rezoning is that sidewalks be installed throughout the interior of the development site to provide a continuous sidewalk network between all the commercial buildings on the site. Five-foot wide sidewalks are shown throughout the interior of the site to provide pedestrian connections to each of the buildings and future outlots on the site. This will allow customers to park once and walk safely between multiple businesses during their visit. One minor correction to the sidewalk network plan is noted: a future sidewalk segment should be illustrated along the areas labeled future outlots on the site plan. These segments will not have to be constructed until those areas are developed.

Interior sidewalk plan is acceptable, provided a future sidewalk section is illustrated along the future outlots. The final location of the required sidewalk along W. Ridgeway Avenue is still being determined and will be based on the recommended improvements to W. Ridgeway Avenue.

7) <u>Building Design:</u> The HWY-1, Highway Commercial District, states that all structures established within the district shall be reviewed for architectural compatibility with surrounding structures. Below is a review on the elements that are to be addressed.

Proportion: The relationship between the width and height of the front elevations of adjacent buildings shall be considered in the construction or alteration of a building; the relationship of width to height of windows and doors of adjacent buildings shall be considered in the construction or alteration of a building.

The scale and proportion of the new retail building and convenience store will be similar to the existing businesses located nearby. There are several hotels nearby with heights varying between two and four stories, with several commercial and industrial buildings in the area that are one story in height. Both the retail store and convenience store will be one story in height. The size of the retail store (185,000 square feet) is larger than most buildings in this area, however because it is on a very large site, the size would not appear to be out of character for the area.

Roof shape, pitch, and direction: The similarity or compatibility of the shape, pitch, and direction of roofs in the immediate area shall be considered in the

construction or alteration of a building.

The design of the both the retail store and convenience store incorporates a flat roof with a parapet wall. The Kwik Star convenience store directly to the north utilizes a similar roof design, while the nearby hotels use a gable roof design. Nearby industrial buildings located within the industrial park utilize a similar flat roof design as well, so this roof design will not be out of character with the area.

Pattern: Alternating solids and openings (wall to windows and doors) in the front facade and sides and rear of a building create a rhythm observable to viewers. This pattern of solids and openings shall be considered in the construction or alteration of a building.

The retail building was designed with textured precast concrete panels in two different tones of gray and with different patterns etched into them to provide some visual interest to the long building walls. This pattern carries through the entirety of the building. The convenience store was similarly designed with the textured concrete precast panels in the two different tones of gray, in order to give it a similar look to the large retail building. The primary façade of the big box store has alternating pattern of window and main entrance features that provide views and openings into the building. These are alternated with the precast concrete panels, separate modules of phenolic panels, some with an aged cedar wood appearance and some in Fleet Farm Orange. Decorative metal awnings also help to visually break up the long facades. All of these elements provide a visually interesting rhythm to the primary façade. The rear and sides of the store will feature mainly the precast concrete panels, along with several overhead doors and service doors. The south side of the building will also feature an auto repair area. There are no façade variations along the rear and sides of the building, however these areas will not be highly visible to neighboring properties to the west because of a large landscaped berm that will be located along the western property line, or the public right-of-way to the south due to the location of future retail buildings and the large amount of trees that will remain along the drainage way at the north end of the property.

The primary façade of the convenience store has an alternating pattern of windows, two types of textured precast concrete panels, and Fleet Farm Orange phenolic panels. These features provide a visually pleasing main entrance into the building. The sides and rear of the building do not have windows due to the location of the attached car wash, however these facades are patterned with the two types of textured precast concrete panels, in addition to the Fleet Farm Orange phenolic paneling which rises above the main roof line to give the building added visual depth.

Materials and texture: The similarity or compatibility of existing materials and textures on the exterior walls and roofs of buildings in the immediate area shall be considered in the construction or alteration of a building. A building or alteration shall be considered compatible if the materials and texture used are appropriate in the context of other buildings in the immediate area.



nolic panels in Fleet

The front of the retail store will feature the two tones of textured concrete precast panels, as well as a white metal perforated panel with the company's name and logo located above the main entrance. Phenolic panels in an aged cedar wood color will be

installed just to the south of the main entrance to give it a more modern look and feel. Also, at the northeast corner of the building will be Fleet Farm Orange phenolic paneling that that wraps around the corner of the building and will feature the company logo. The retail store will also feature a yard area at the south end of the building, which will be surrounding by a 16' tall wood fence at the south side and an 8' tall metal/slatted chain link fence on the east and west sides. Staff notes that the wood fence should be stained or painted to provide a more finished look visible from Highway 20 and to prevent deterioration. The south side of the store will feature an auto repair area, so several large overhead doors will be located on this side.



Yard Fence at Barn - 16'

Convenience Store

The convenience store will also feature the two tones of textured precast concrete panels, as well as the Fleet Farm Orange phenolic paneling located along portions of all four sides of the building.

Color: The similarity or compatibility of existing colors of exterior walls and roofs of buildings in the area shall be considered in the construction or alteration of a building.

Many of the existing buildings in this area utilize a neutral color exterior, which include brown, tan, and cream. Some buildings also utilize red or gray tones as well. The retail store and convenience store will include two shades of gray in the textured precast concrete panels, with areas of the signature Fleet Farm Orange highlighted on several areas of the buildings. Staff feels that the amount of the orange that is incorporated into the two buildings does not take away from the overall look of the development and provides additional visual interest to the facades.

Architectural features: Architectural features, including but not limited to, cornices, entablatures, doors, windows, shutters, and fanlights, prevailing in the immediate area, shall be considered in the construction or alteration of a building. It is not intended that the details of existing buildings be duplicated precisely, but those features should be regarded as suggestive of the extent, nature, and scale of details that would be appropriate on new buildings or alterations.

Architectural features of the retail store include two large curtain walls of windows on the front of the building, along with the raised perforated metal panel located above the main entrance. The convenience store will have typical windows located on it, and also incorporates the Fleet Farm Orange phenolic panels on it to give it contrast from the gray textured concrete precast panels.

Overall, the design of the retail store and convenience store is architecturally compatible with other buildings in the surrounding area.

- 8) <u>Trash Dumpster Site:</u> A trash compactor will be located within the Fleet Farm building near the truck loading docks at the southwest corner of the building. Also, a trash dumpster enclosure is located at the north end of the convenience store parking lot. This enclosure will be constructed with textured precast concrete, with a color matching gate. The color of the enclosure will match the color of the convenience store building. **Dumpster enclosure plan is acceptable.**
- 9) <u>Lighting Plan:</u> The HWY-1 District regulations do not have specific lighting design guidelines. The site plan shows the location of light poles and all wall lights throughout the site. The parking lot lights will be mounted atop 38' tall light poles and will include a single head fixture. These fixtures will be housed in a die-cast aluminum housing with



LED lights. Also, wall mounted lights will be located on the walls of the building in various locations, and surface mounted downlights will be located under the petroleum canopy. **Lighting plan is acceptable.**

10) Signage: Three (3) monument signs are illustrated on the site plan in different locations on the property. The main sign (as shown to the right), located near the south end of the property along U.S. Highway 20, will be 25 feet in height and 200 square feet in area. The sign will sit on a stone veneer base that matches the color of the building. Below the sign lettering will be an LED reader board for messaging.

Two smaller 15' tall signs will be located near the corner of Highway 58 and W Ridgeway Avenue and near the eastern entrance to the property along W Ridgeway Avenue. One of the signs will be 150 square feet in area and



the other will be 118.6 square feet in area. These signs will also have a stone veneer base that matches the color of the building, but will not have an LED reader board.

It should be noted that the property is located within the Highway 20 Commercial Corridor Overlay Zoning District. The signage requirements in this district state that one freestanding sign may be allowed that does not exceed 25 feet in height and 200 square feet in area. The main monument sign would meet those requirements. The ordinance goes on to state that smaller monument signs, measuring no more than 15 feet in height and 150 square feet in area, are permitted, with a maximum of two such signs per parcel. The two additional signs on the property would meet these requirements as well.

The proposed wall signs appear to be well within the District limitations of no larger than 20% of the wall area to which the wall sign is attached. However, this will be reviewed in detail at the time a sign permit is requested. **Signage plan is acceptable, subject to detailed review with a sign permit.**

11) Storm Water Management: A total of three (3) storm water detention basins will be located on the property to collect the storm water runoff from the site. Basin #1 as shown on the plan will be located within the main parking lot area, east of the future retail buildings. This basin will collect water from a majority of the development site. The water from this basin will be released at a controlled rate via a pipe into Basin #2. Basin #2 as shown on the plan will be located just west of the convenience store and north of the main parking lot. This basin will collect water from the convenience store, and also the water from Basin #1. The water will then be released at a controlled rate into the drainage way and wetland located along the north side of the property along W Ridgeway Avenue. Basin #3 as shown on the plan will collect water from the remaining southern half of the development. The water will then be released at a controlled rate into the drainage ditch to the east along Highway 58. Stormwater Management Plan is currently being reviewed by the Engineering Department.

TECHNICAL COMMENTS

The rezoning of this property from A-1 to HWY-1 was recommended for approval by the Planning & Zoning Commission on July 25, 2018. As part of that recommendation of approval, five conditions were attached in order to keep the rezoning process moving forward, as the City had not seen a traffic impact study at that time. It should be noted that a traffic impact study was submitted to the City on Monday, October 1, 2018, and is currently under review by City staff and a third party. The first condition was that all street, intersection, and traffic control improvements necessary to provide for safe and efficient traffic control and circulation to serve the long term needs of the subject development at full build out without causing undue traffic circulation and congestion problems be designed and agreed upon prior to approval of the rezoning at City Council and that said improvements must be constructed and accepted by the City prior to issuance of an occupancy permit for any portion of the development. To date, this has not been designed and agreed upon because the City is still reviewing the traffic study. The design of the improvements should be agreed upon by the City and developer by the time of the next Planning & Zoning Commission meeting on October 24th. The second condition was area shown as "Future R/W (right-of-way)" on the concept site plan shall remain as open space and shall not be developed with any structures, fences, buildings, hard surfacing, driveways or sidewalks. This open space is shown on the site plan, so this condition has been addressed. The third condition was that and when the property to the west ever redevelops with commercial uses, a cross-access drive shall be constructed by the property owner at their expense within a 30-foot wide cross-access easement that will be established at the time of site plan approval. The exact location of the easement will be determined with the site plan. The site plan does show a 30-foot wide cross access easement just to the north of the retail building, however, this easement will need to be recorded. This drive would not need to be constructed until the property to the west redevelops. The fourth condition was that 5-foot wide sidewalk shall be installed along the entire frontage of the property along W. Ridgeway Avenue. The City will work with the developer to determine the best location for the easternmost sidewalk segment to avoid the wetland and provide for safe pedestrian access to the corner of Ridgeway and Hwy 58. The site plan shows a 5' wide sidewalk along the W Ridgeway Avenue street frontage; however staff is concerned with how close the sidewalk gets to W Ridgeway Avenue as you go east of Drive #2 across from Nordic Drive. Additional analysis may be needed to see if there is a more suitable, safer location for this sidewalk. The fifth condition was that sidewalks shall be installed throughout the interior of the development site to provide a continuous sidewalk network between all the commercial buildings on the site. The site plan shows interior sidewalks that lead from the main retail building to the future retail buildings to the north and east to the convenience store. As noted in the staff report, a future sidewalk segment should be shown across the frontage of the area labelled as future outlots. This will not have to be installed until that are is developed, but should be shown on the plan.

Several technical comments were made by Cedar Falls Utilities staff regarding utility locations, and the Engineering Division has made technical comments regarding sanitary sewer and storm water facilities. These comments were sent to the developer to be addressed.

Water, electric, gas, and communications utility services are available to the site in accordance with the service policies of Cedar Falls Utilities. The property owner/contractor is responsible to extend all utility services to the building. These utility extensions will be reviewed by CFU personnel as part of the building plan review.

STAFF RECOMMENDATION

The introduction of this site plan is for discussion and public comment purposes only. The Community Development Department has reviewed the plan and provides the following comments:

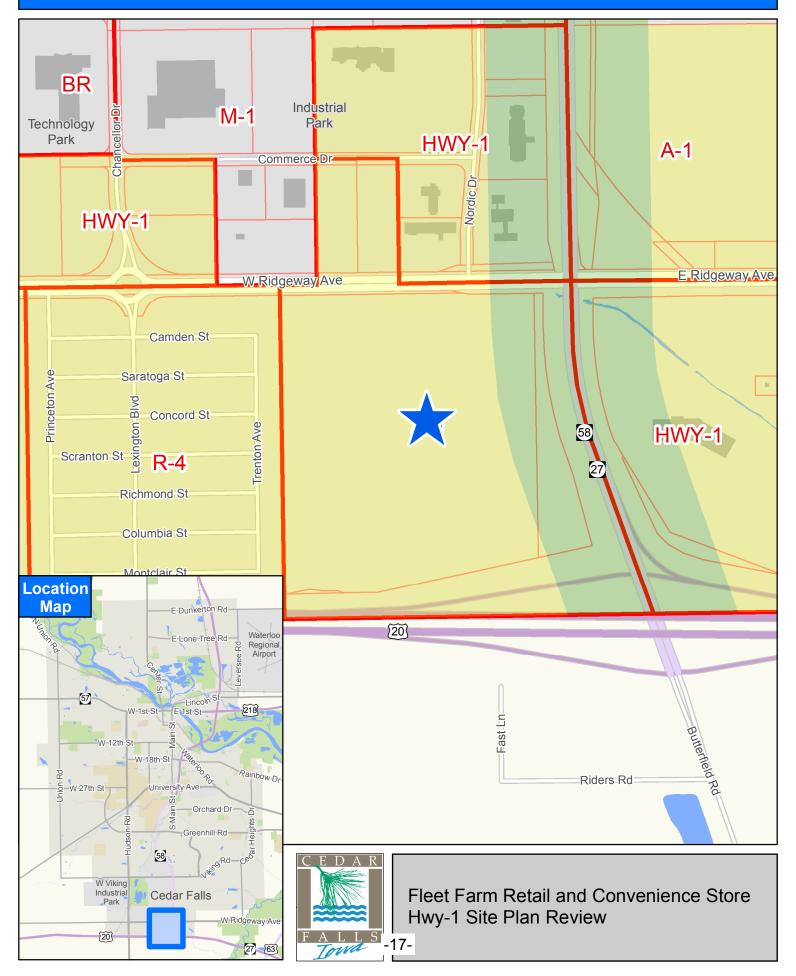
- 1) Review of traffic study to determine the public roadway improvements necessary for this development.
- 2) Review of the location of the sidewalk along W Ridgeway Avenue.
- 3) Corrections needed based on technical comments.
- 4) Any comments or direction specified by the Planning & Zoning Commission.

Subject to the issues and comments noted above being addressed, staff anticipates that this will be referred to the Planning and Zoning Commission for a vote on October 24, 2018.

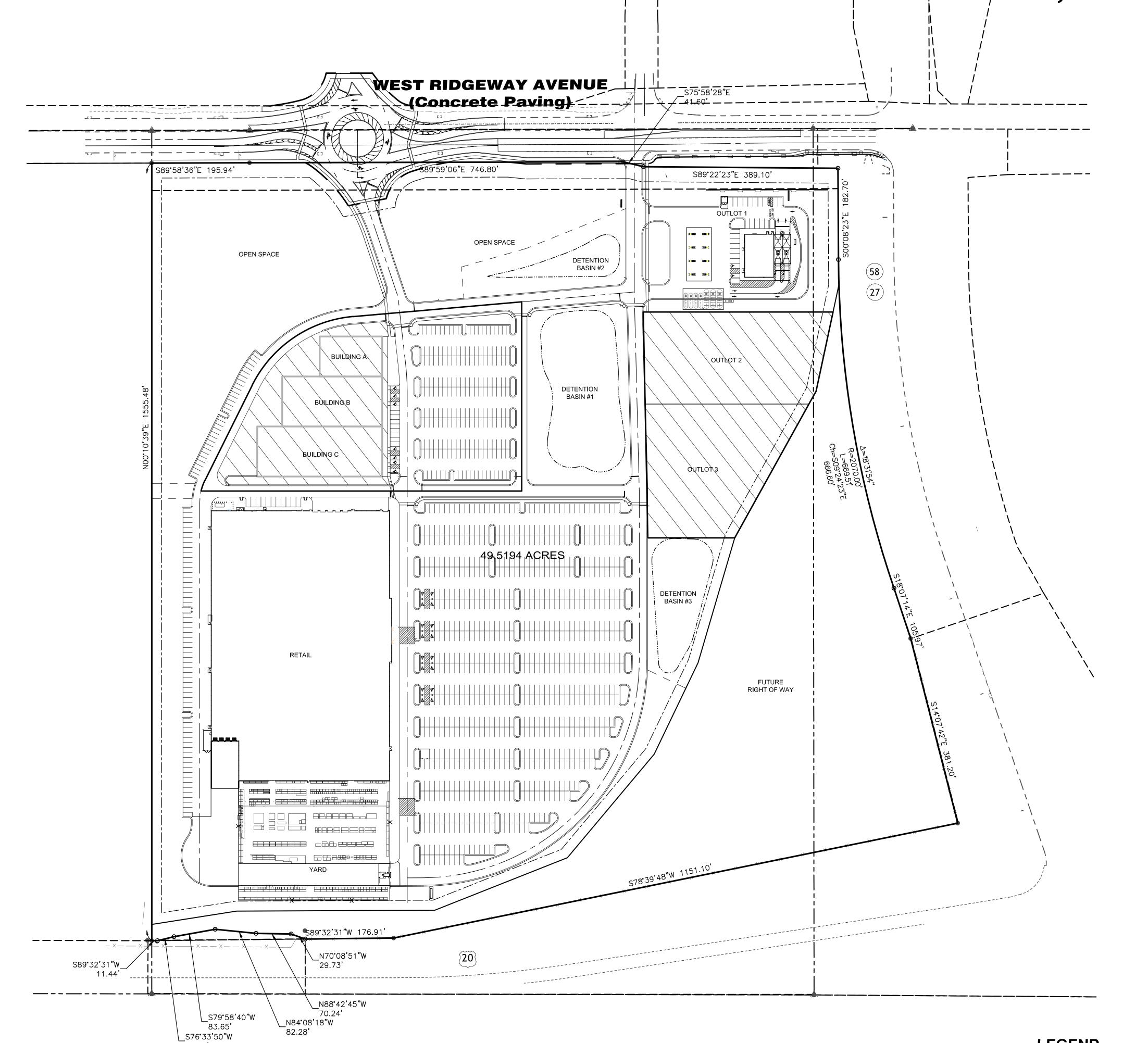
PLANNING & ZONING COMMISSION

Discussion 10/10/2018 Vote 10/24/2018

Cedar Falls Planning & Zoning Commission October 10, 2018



CEDAR FALLS, IOWA 50613 REVISED SEPTEMBER 28, 2018



PROJECT VICINITY MAP



1300 S. LYNNDALE DRIVE

DEVELOPER

MIDLAND ATLANTIC PROPERTIES 8044 MONTGOMERY RD, SUITE 370 CINCINNATI, OH 45236 513-792-5000

LAND PLANNER/ **ENGINEER**/ LANDSCAPE ARCHITECT

BAYER BECKER, INC. 6900 TYLERSVILLE ROAD, SUITE A MASON, OH 45040

SURVEYOR

513-336-6600

VJ ENGINEERING 1501 TECHNOLOGY PARKWAY, SUITE 100 CEDAR FALLS, IOWA 50613

ARCHITECT

1220 MARSHALL STREET N.E. MINNEAPOLIS, MN 55413 612-677-7100

BENCHMARK

SANITARY MANHOLE NORTHEAST SIDE OF PROPERTY CORNER AND SOUTH OF WEST RIDGEWAY AVENUE T/RIM=913.99 12"(E&W)INV=900.15 12"(S)INV=901.25

UTILITIES CONTACTS:

JOHN OSTERHAUS (319) 268-5298 WATER & GAS CEDAR FALL UTILITIES

ELECTRIC & TELEPHONE CEDAR FALL UTILITIES

JERALD LUKENSMEYER (319) 266-1761 STORM & SANITARY SEWERS
CITY OF CEDAR FALLS 220 CLAY ST. CEDAR FALLS, IA 50613

SITE SUMMARY

HWY-1 WITH HWY-20 OVERLAY HWY-1 WITH HCG OVERLAY (OUTLOT 1

36.563 ACRES

OPEN SPACE ACREAGE: 12.107 ACRES 33.1% (DEVELOPMENT AREA) 24.4% (TOTAL AREA)

GROSS LEASABLE AREA: 240,000 S.F. RETAIL GROSS LEASABLE AREA (GLA): 185,000 S.F. FUTURE JUNIOR ANCHORS (JA) - BUILDINGS A,B,C - GLA: 55,000 S.F.

REQUIRED PARKING SPACES: RETAIL PARKING: 832.5 (4.5 SPACES/1000 S.F. GLA) TYPICAL PARKING DIMENSIONS - RETAIL: 10' X 20'

JA PARKING: 247.5 SPACES (4.5 SPACES/ 1000 S.F. GLA) TYPICAL PARKING DIMENSIONS - JA: 9' X 19' (COMPACT 8' X 19')

PROVIDED PARKING SPACES:

STANDARD 833 SPACES 16 SPACES 849 SPACES **OUTLOT 1 (FUEL CENTER)** STANDARD 23 SPACES **ACCESSIBLE** 2 SPACES 25 SPACES

JA (FUTURE DEVELOPMENT) 240 SPACES **ACCESSIBLE** 7 SPACES 247 SPACES

OVERALL SITE (RETAIL + JA) 1,096 SPACES 4.5 SPACES / 1000 S.F.

APPROXIMATE 150 TO 200 EMPLOYEES

ENGINEER'S CERTIFICATION

I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly licensed professional engineer under the laws of the State of Iowa.

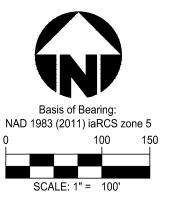
Gregory J Koch, P.E. lowa License No. 18441 My license renewal date is December 31, 2018

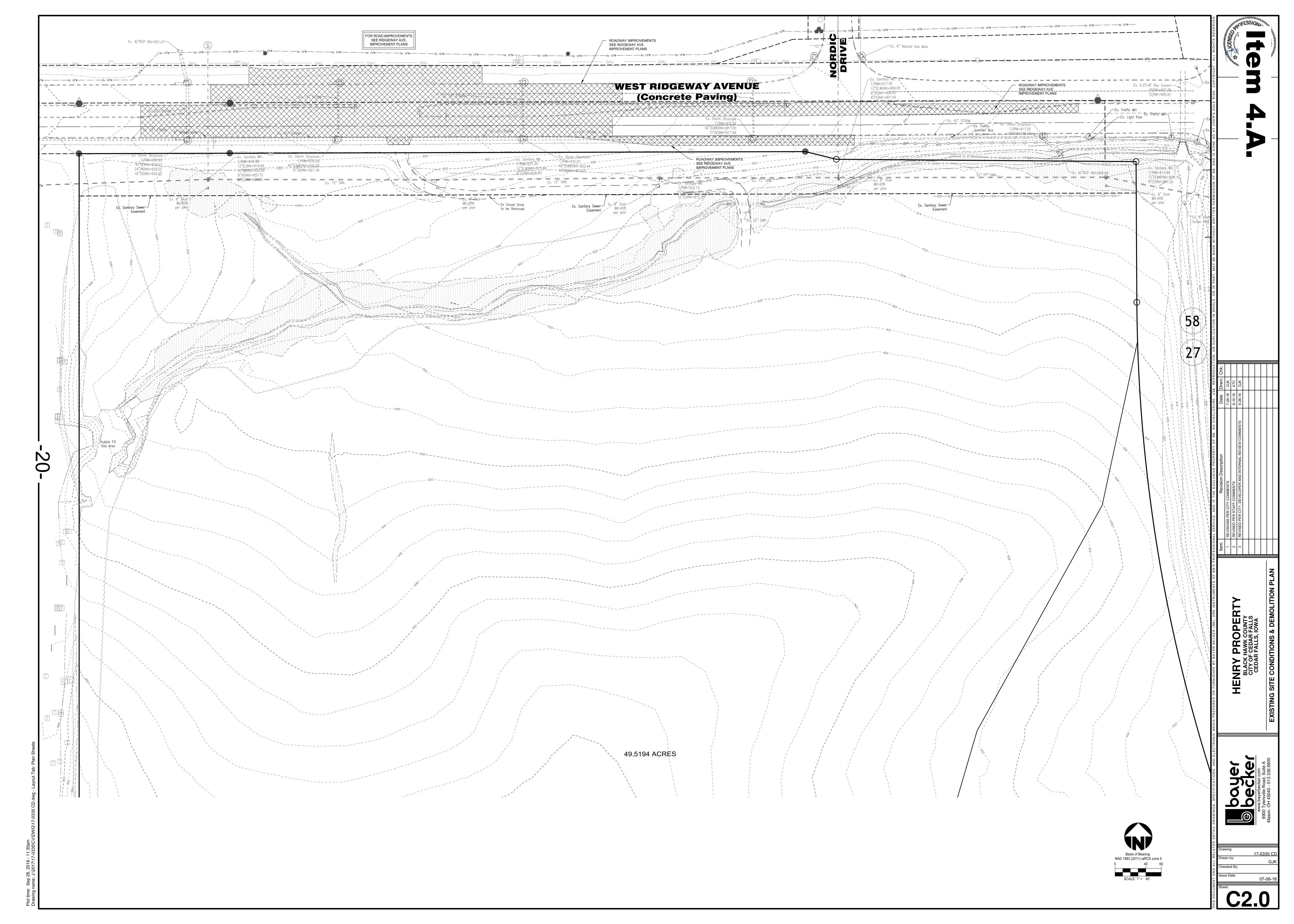
Pages or sheet covered by this seal: C1.0, C2.0, C2.1, C3.0, C3.1, C4.0, C4.1, C4.2, C5.0, C5.1, C6.0, C6.1, C6.2, C7.0, AND C7.1.

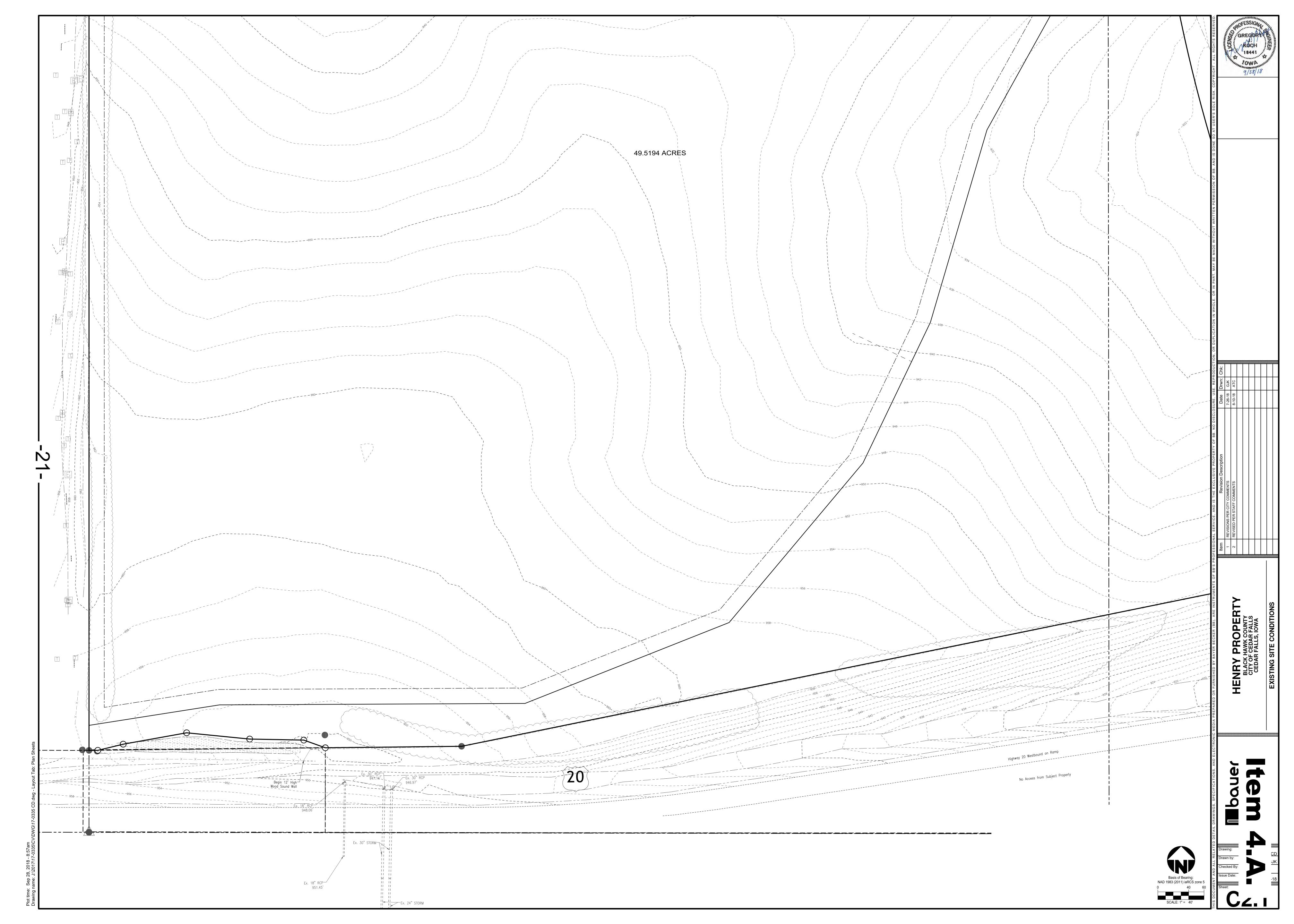
INDEX OF SHEETS

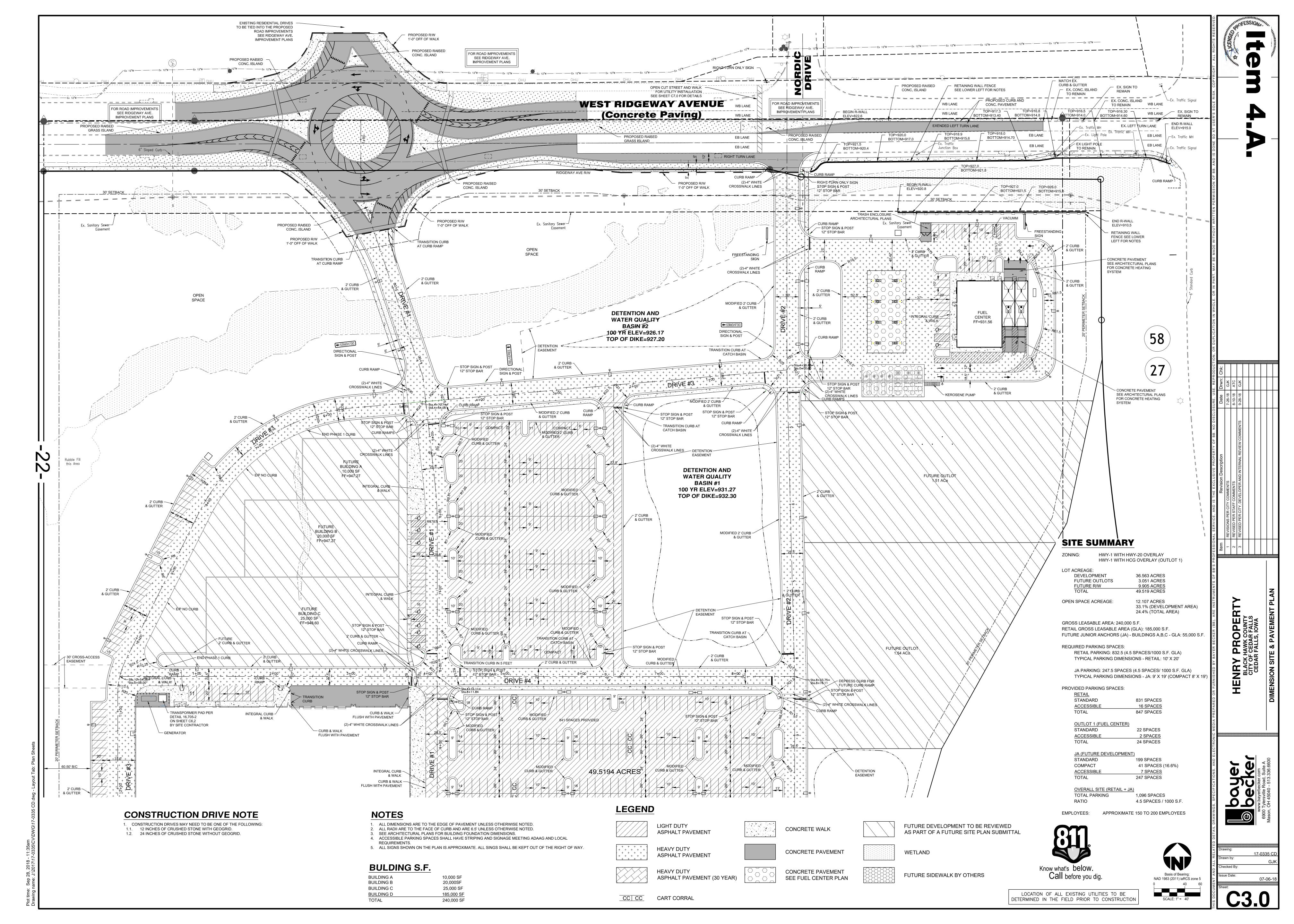
DRAWING NO.	DRAWING TITLE	ISSUE DATE	REVISION NO.	REVISION DATE
C1.0	TITLE SHEET	07-06-18	3	09-28-18
C2.0	EXISTING SITE CONDITIONS	07-06-18	3	09-28-18
C2.1	EXISTING SITE CONDITIONS	07-06-18	2	08-10-18
C3.0	DIMENSION SITE & PAVEMENT PLAN	07-06-18	3	09-28-18
C3.1	DIMENSION SITE & PAVEMENT PLAN	07-06-18	3	09-28-18
C4.0	UTILITY PLAN	07-06-18	3	09-28-18
C4.1	UTILITY PLAN	07-06-18	3	09-28-18
C4.2	UTILITY PROFILES	07-06-18	2	09-28-18
C5.0	GRADING PLAN	07-06-18	3	09-28-18
C5.1	GRADING PLAN	07-06-18	2	09-28-18
C6.0	EROSION & SEDIMENT CONTROL SITE PLAN	09-14-18		
C6.1	EROSION & SEDIMENT CONTROL SITE PLAN	09-14-18		
C6.2	SEDIMENTATION & EROSION CONTROL DETAILS	07-06-18	2	09-28-18
C7.0	SITE AND PAVEMENT DETAILS	07-06-18	2	09-28-18
C7.1	STORM SEWER DETAILS	07-06-18	2	09-28-18
C7.2	STORM AND SANITARY SEWER DETAILS	07-06-18	1	07-26-18
L1.0	OPEN SPACE EXHIBIT & OVERALL PLANTING PLAN	07-06-18	3	09-28-18
L1.1	PLANTING PLAN - NORTH	07-06-18	3	09-28-18
L1.2	PLANTING PLAN - SOUTH	07-06-18	3	09-28-18
L1.3	SEEDING & MULCHING PLAN	07-26-18	2	09-28-18
L2.0	PLANTING NOTES & DETAILS	07-06-18	1	07-26-18

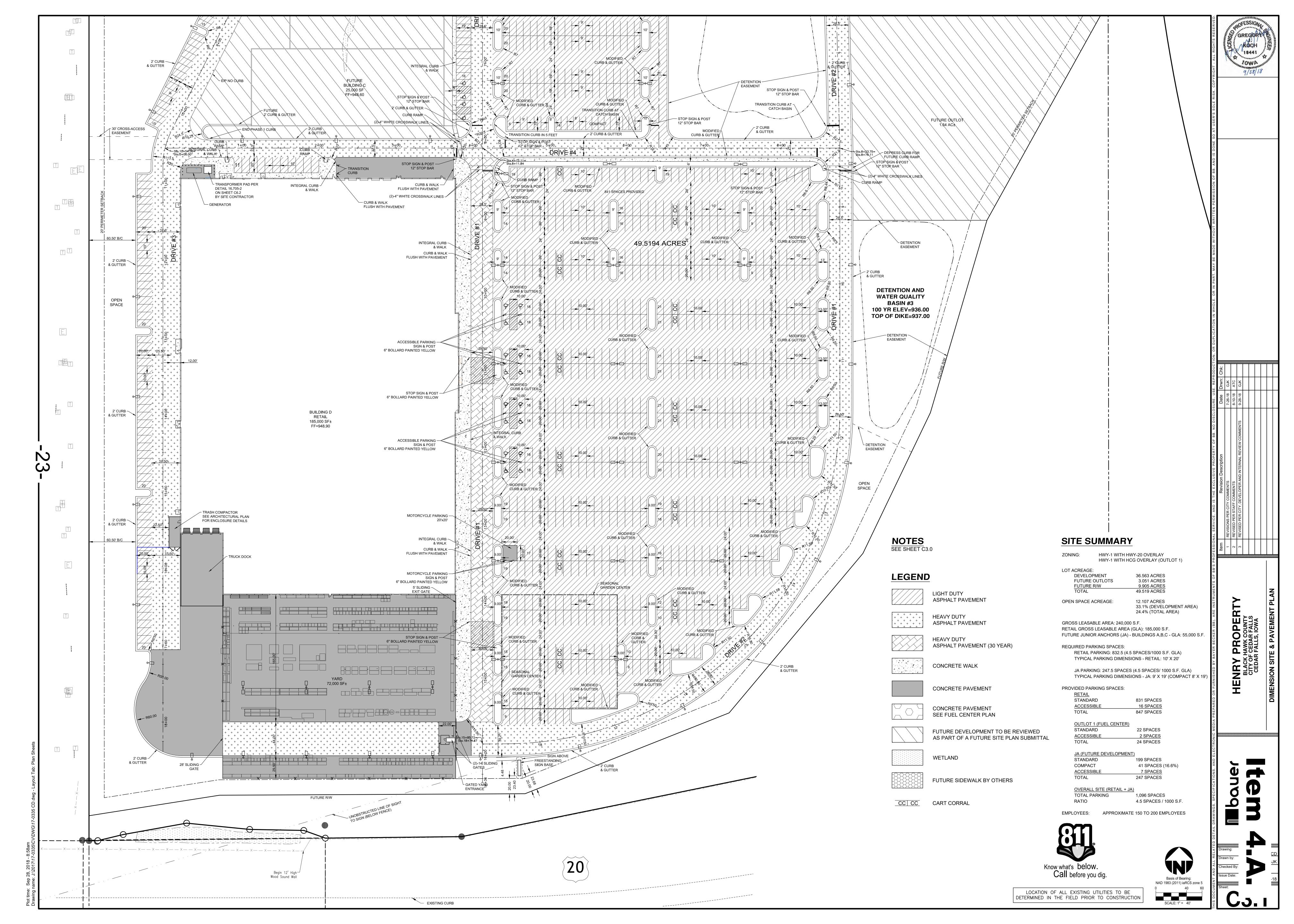
FUTURE DEVELOPMENT

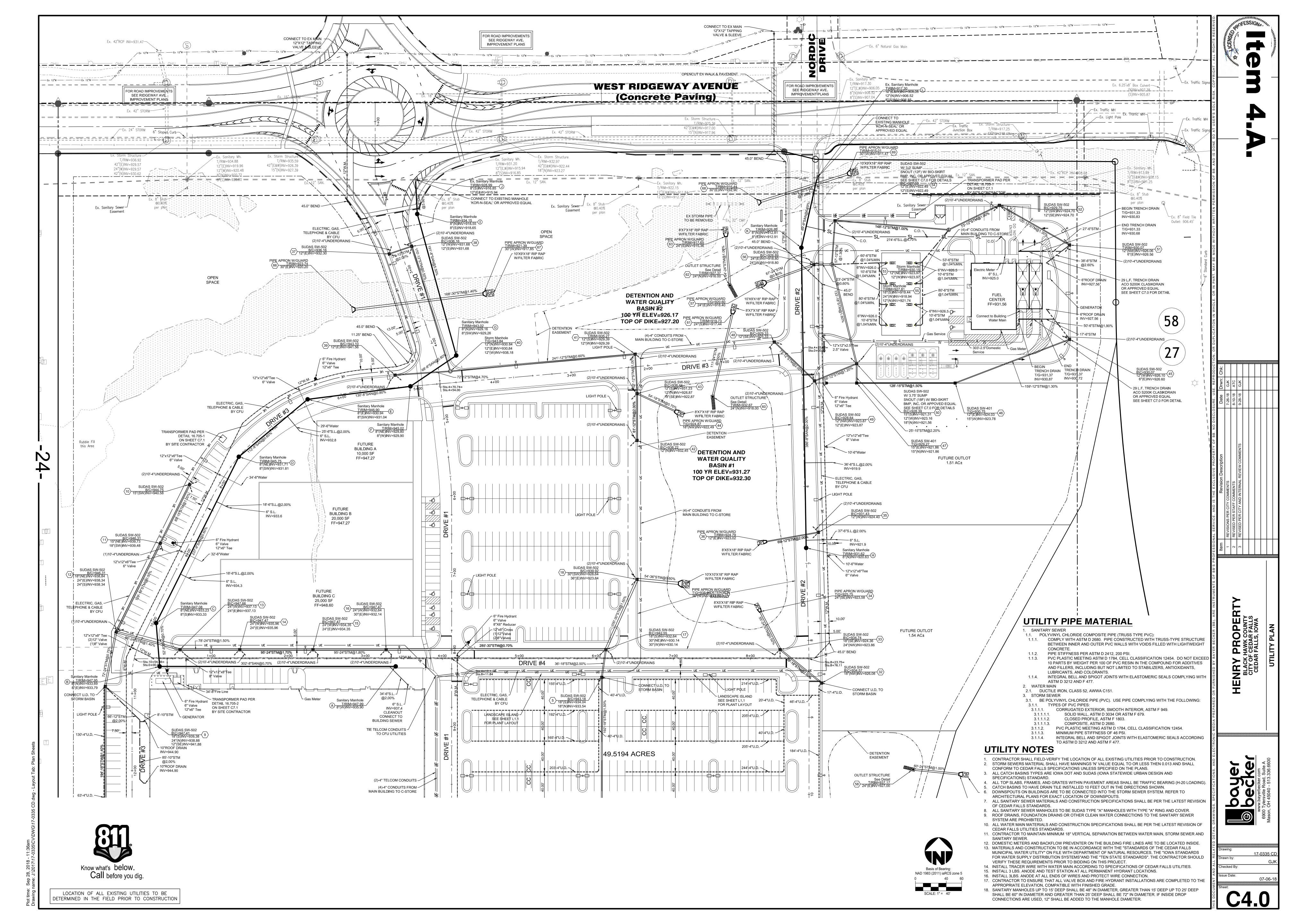


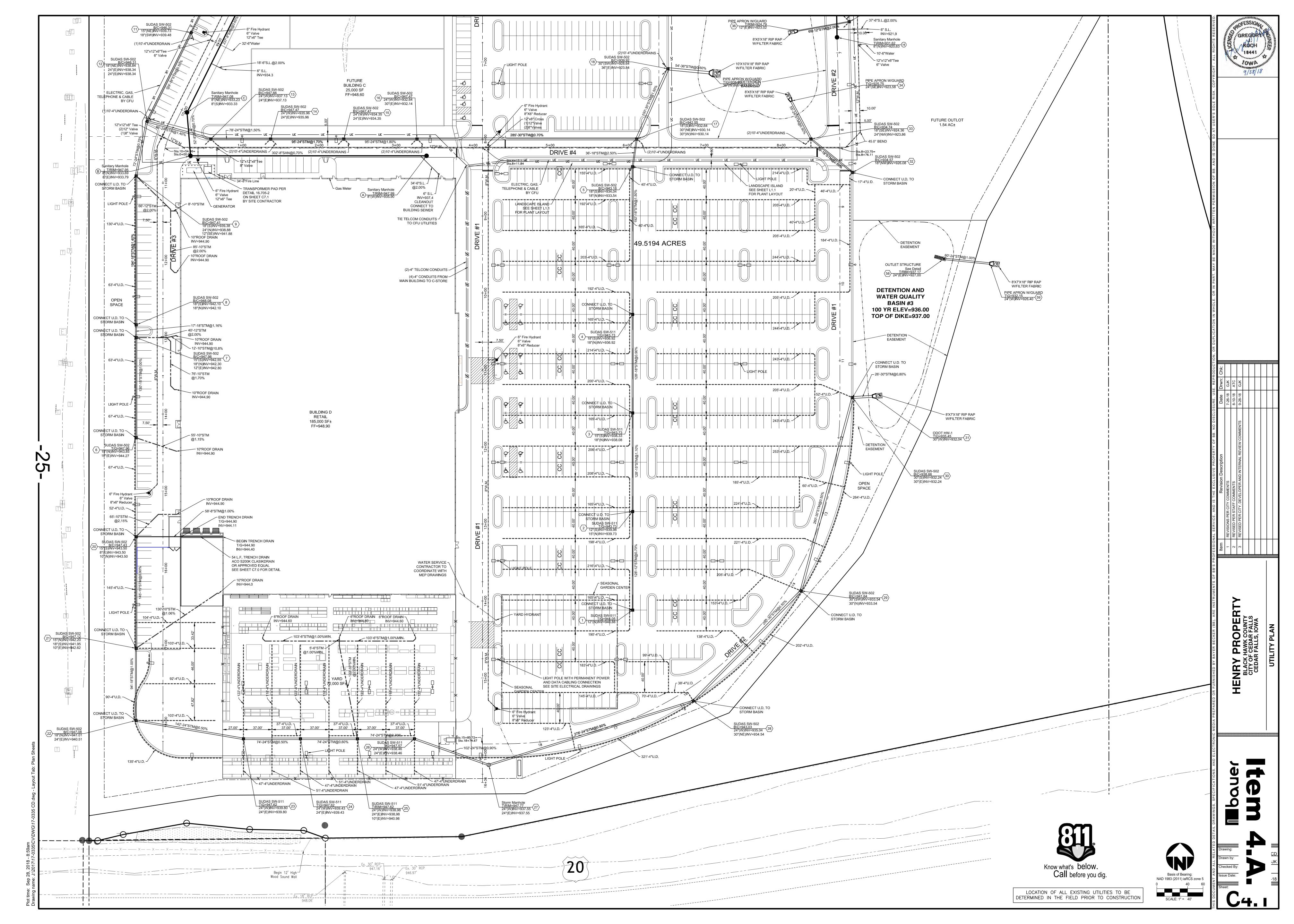


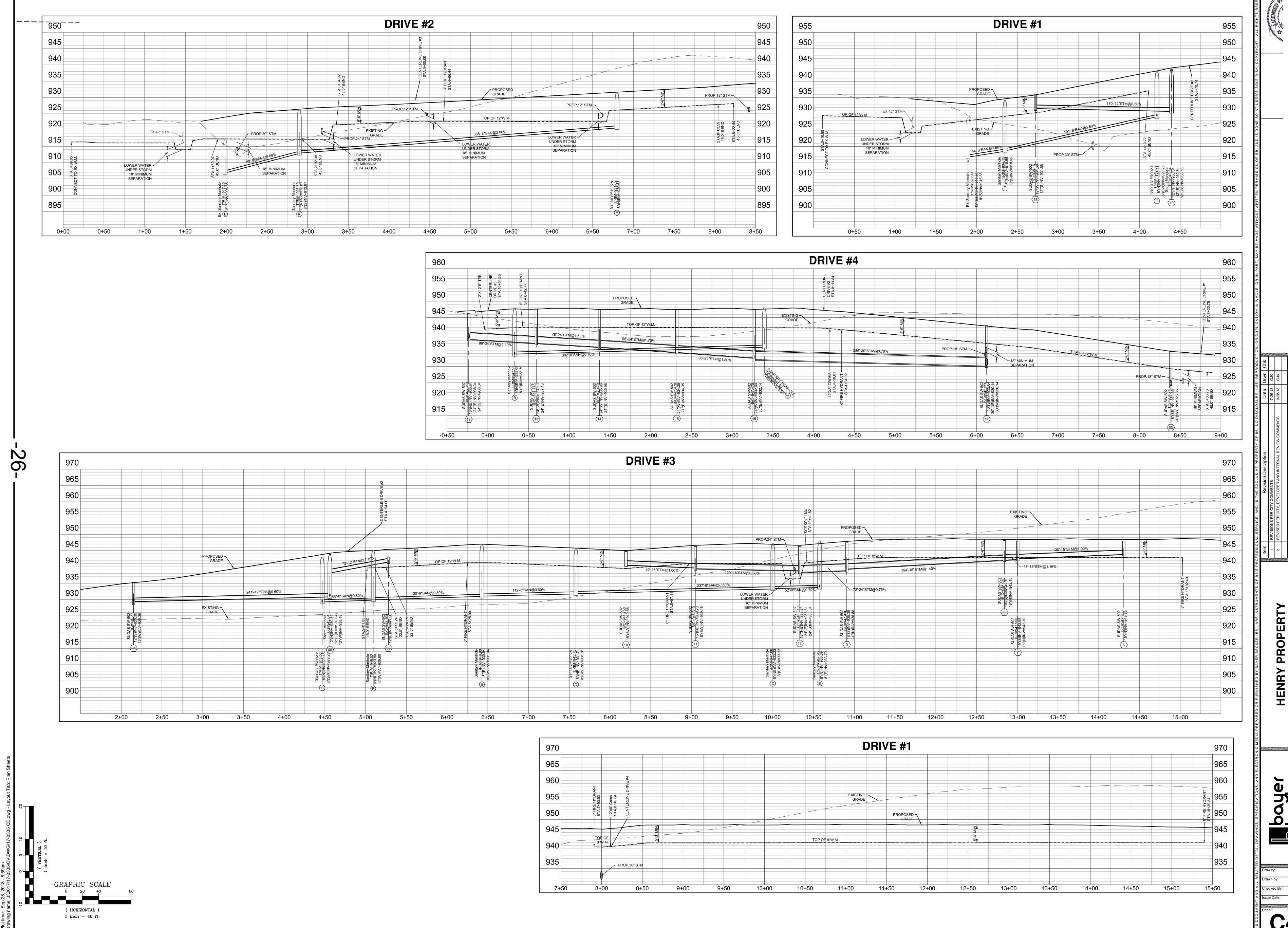




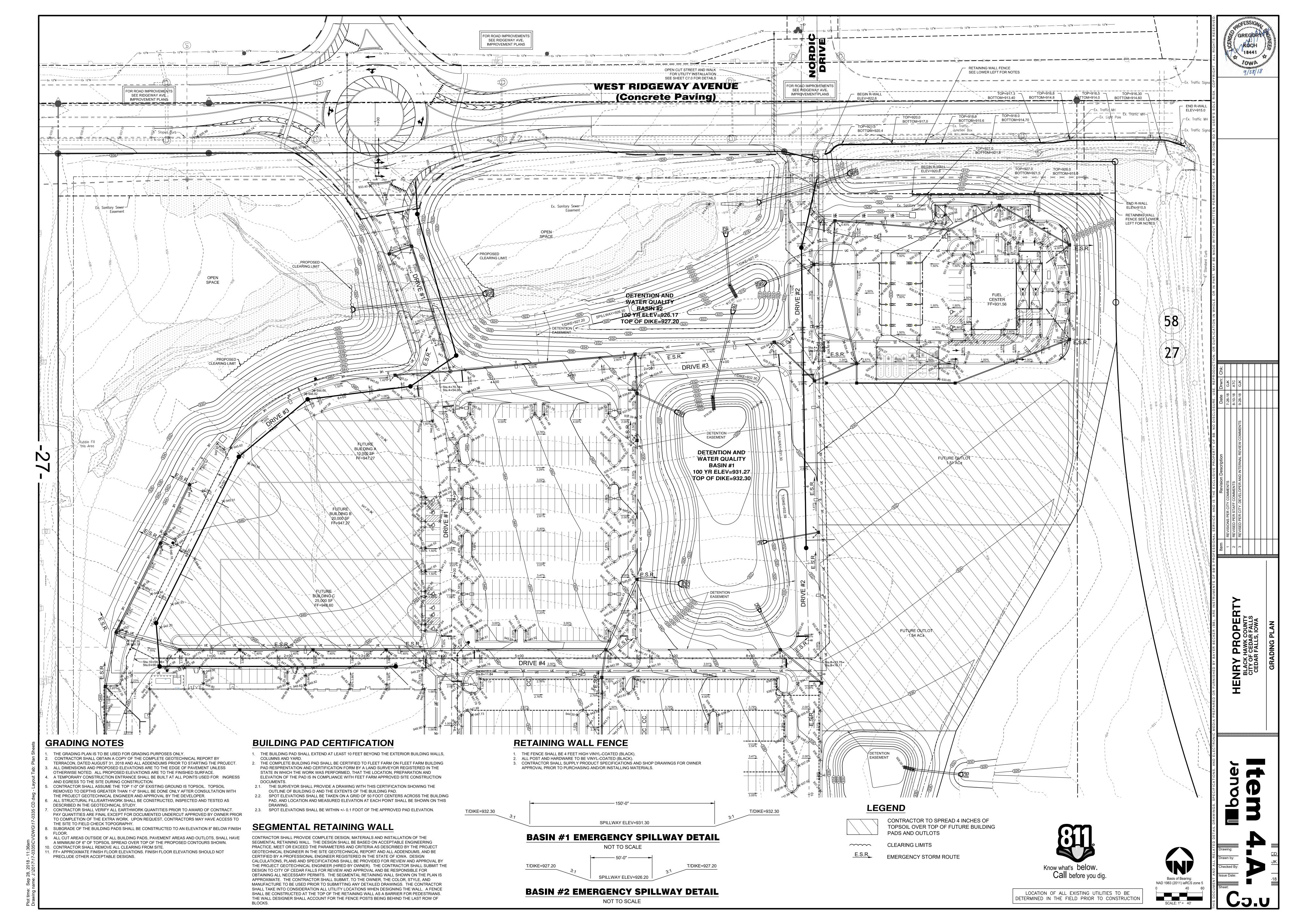


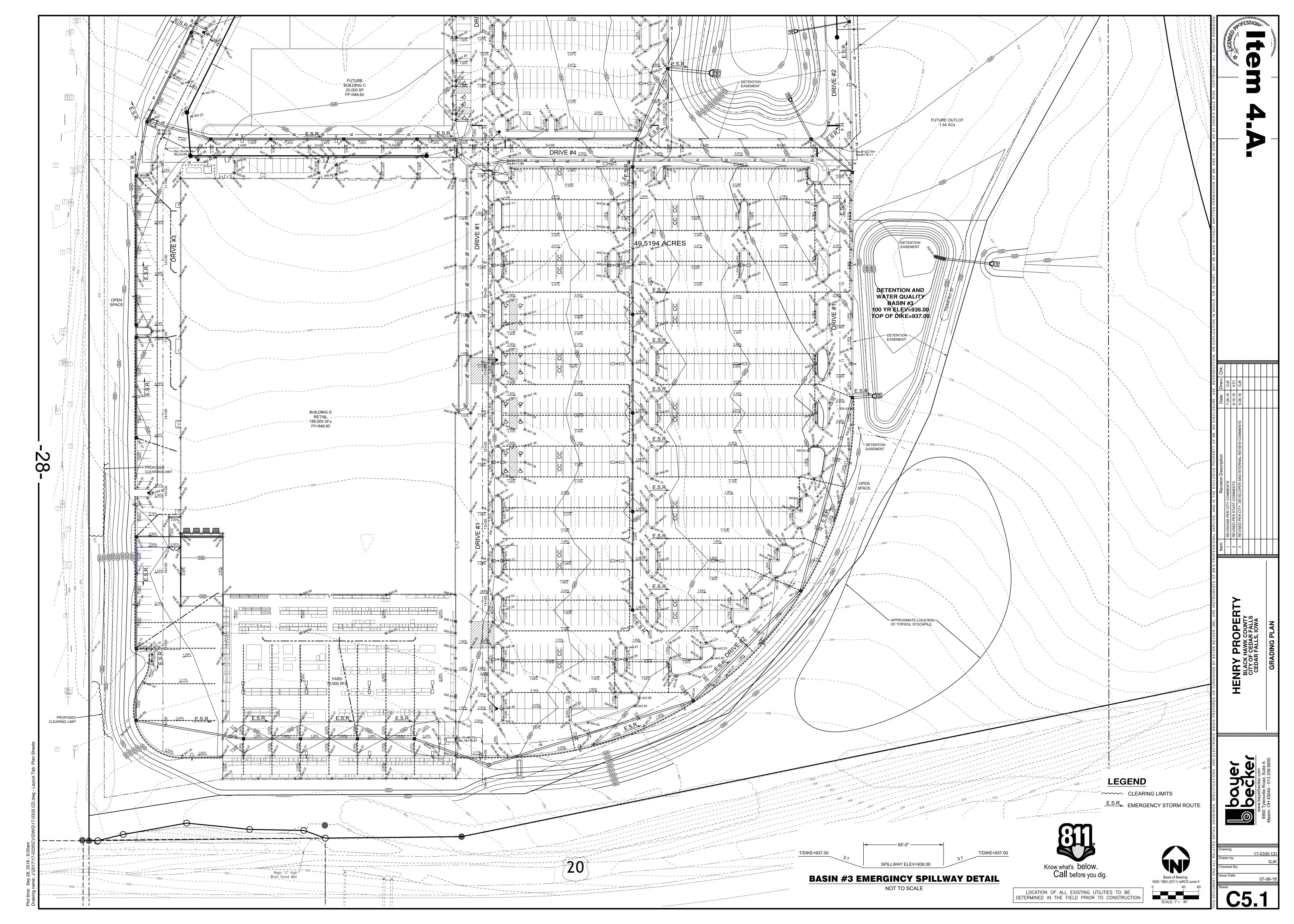


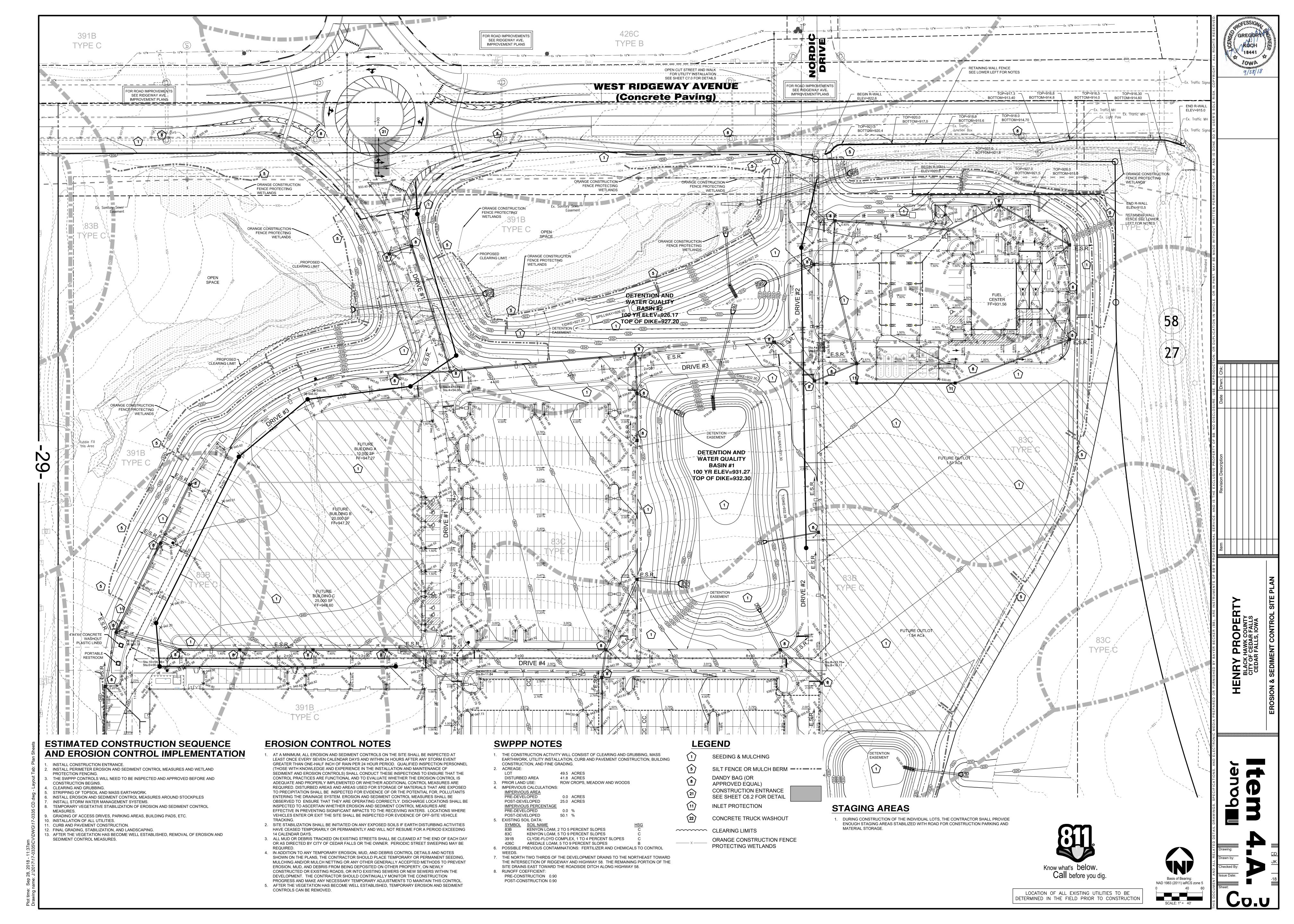


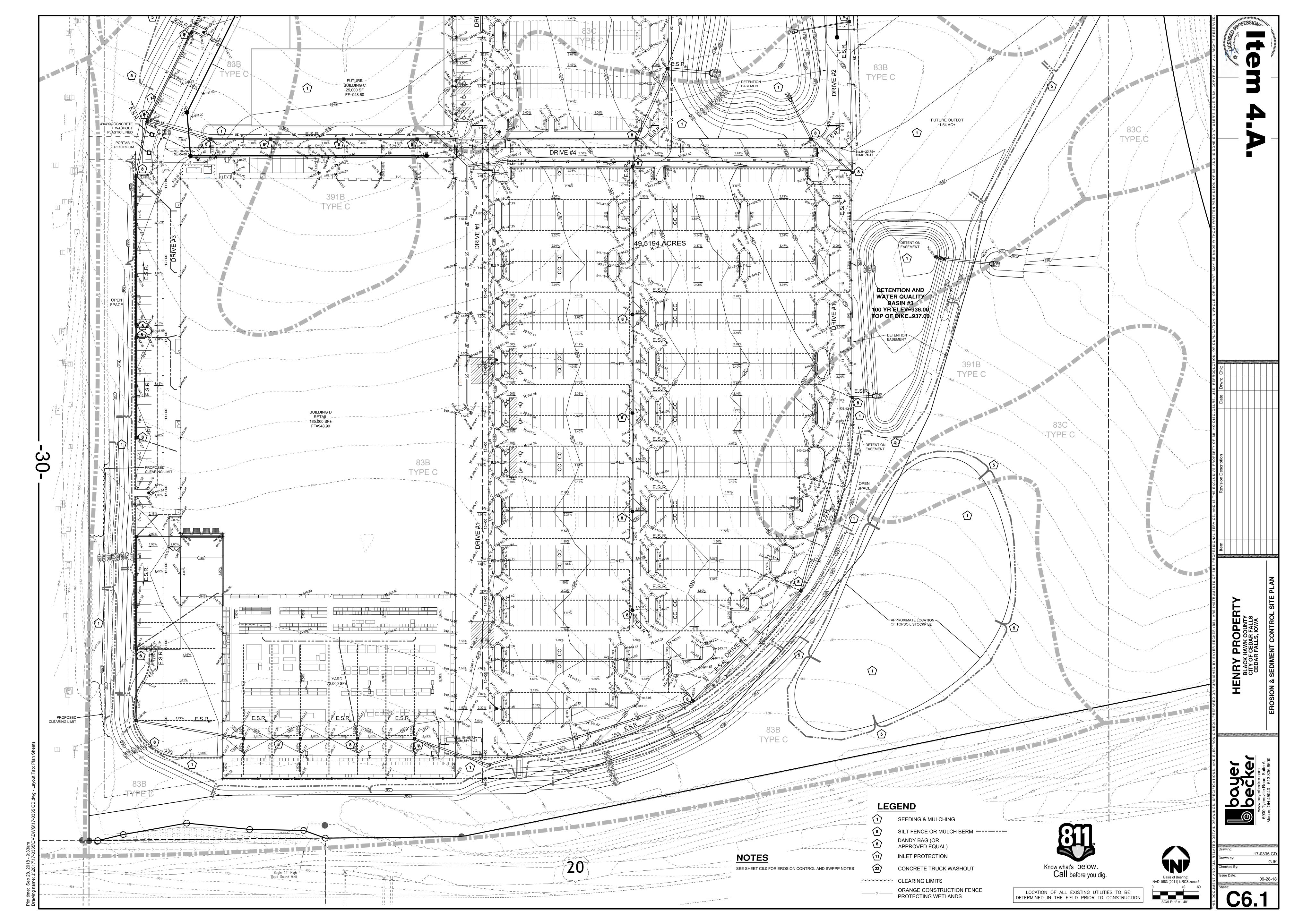


PROPERTY HAWK COUNTY F CEDAR FALLS R FALLS, IOWA









CONDITIONS WHERE PRACTICE APPLIES

- Permanent seeding should be applied to: * Areas or portions of construction-sites which can be brought to final grade. Applications of permanent seeding should not be delayed while construction on limited portions of the site
- * Areas on that will be regraded, but will be dormant for a year or more.
- PLANNING CONSIDERATIONS
- Healthy dense turf will have a dramatic long lasting effect on stormwater quality as well as promoting infiltration and reducing the amount of runoff. To establish quality vegetation, careful preparation of the seedbed, soil, even subsoil is highly encouraged.
- Soil Compaction--Stormwater quality and the amount of runoff both vary significantly with soil compaction. Non-compacted soils improve stormwater by promoting:
- dense vegetation. high infiltration & lower runoff rates.
- pollutant filtration, deposition & absorption, and beneficial biologic activity in the soil.
- Construction activity can cause highly compacted soils but also offers the opportunity to improve soil condition. The best time for improving soil condition is during the establishment of permanent vegetation. It is highly recommended that subsoilers, plows or others implements be specified as part of final seedbed preparation. Use discretion in slip-prone areas.
- Minimum Soil Conditions--Vegetation cannot be expected to stabilize soil that is unstable due to its texture, structure, water movement or excessively steep slope. The following minimum soil conditions are needed for the establishment and maintenance of a long-lived vegetation cover. If these conditions cannot be met,
- see the Standards and Specifications for Resoiling. Soils must include enough fine-grained material to hold at least a moderate amount of available moisture. The soil must be free from material that is toxic or otherwise harmful to plant growth.

	Perma	anent Seeding		
Seed Mix	Seed	ling Rate	Notes	
Seed IVIIX	lb./ac. lb./1,000 ft. 2		Notes:	
	Ge	neral Use		
Creeping Red Fescue Ryegrass Kentucky Bluegrass	20-40 10-20 10-20	1/2-1 1/4-1/2 1/4-1/2		
Tall Fescue	40	1		
Dwarf Fescue	40	1		
	Steep Ban	ks or Cut Slopes		
Tall Fescue	40	1		
Crown Vetch Tall Fescue	10 20	1/4 1/2	Do not seed later than August	
Flat Pea Tall Fescue	20 20	1/2 1/2	Do not seed later than August	
	Road D	itches and Swale	S	
Tall Fescue	40	1		
Dwarf Fescue Kentucky Bluegrass	90 5	2 1/4		
		Lawns		
Perennial Ryegrass Kentucky Bluegrass	60 60	1 1/2 1 1/2		
Creeping Red Fescue Kentucky Bluegrass	60 60	1 1/2 1 1/2	For shaded areas	

aintenance for Permanent Seedings ertilization and Mowing

Formula	lb./ac.	lb./1,000 sq. ft.	Time	Mowing
10-10-10	500	12		Not closer than 3"
10-10-10	500	12	Fall, yearly or as needed	Not closer than 4"
10-10-10	500	12		Not closer than 2"
0-20-20	400	10	Spring, yearly following establishment	Do not mow
0-20-20	400	10	and every 4-7 yrs. thereafter	Do not mow
	10-10-10 10-10-10 10-10-10	10-10-10 500 10-10-10 500 10-10-10 500 0-20-20 400	10-10-10 500 12 10-10-10 500 12 10-10-10 500 12 10-20-20 400 10	10-10-10 500 12 Fall, yearly or as needed 10-10-10 500 12 Spring, yearly following establishment and every 4-7 yrs.

SITE PREPARATION

- A subsoiler, plow or other implement shall be used to reduce soil compaction and allow maximum infiltration. (Maximizing infiltration will help control both runoff rate and water quality.) Subsoiling should be done when the soil moisture is low enough to allow the soil to crack or fracture. Subsoiling shall not be done on slip-prone areas where soil preparation should be limited to what is necessary for establishing
- The site shall be graded as needed to permit the use of conventional equipment for seedbed preparation and seeding.
- Resoil shall be applied where needed to establish vegetation.
- SEEDBED PREPARATION
- Lime--Agricultural group limestone shall be applied to acid soil as recommended by a soil test. In lieu of a soil test, lime shall be applied at the rate of 100 lb./1,000 sq. ft. or 2 tons/ac.
- Fertilizer--Fertilizer shall be applied as recommended by a soil test. In lieu of a soil test, fertilizer shall be applied at a rate of 12 lb./1,000 sq. ft. or 500 lb./ac. of 10-10-10- or 12-12-12 analysis.
- The lime and fertilizer shall be worked into the soil with a disk harrow, spring-tooth harrow, or other suitable field implement to a depth of 3 in. On sloping land the soil shall be worked on the contour. SEEDING DATES AND SOIL CONDITIONS
- Seeding should be done March 1 to May 31 or August 1 to September 30. These seeding dates are ideal but, with the use of additional mulch and irrigation, seedings may be made any time throughout the growing season. Tillage/seedbed preparation should be done when the soil is dry enough to crumble and not form ribbons when compressed by hand. For winter seeding, see the following section on dormant seeding.

- Mulch material shall be applied immediately after seeding. Seedings made during optimum seeding dates and with favorable soil conditions and on very flat areas may not need mulch to achieve adequate stabilization. Dormant seeding shall be mulched.
- Straw--If straw is used it shall be unrotted small-grain straw applied at the rate of 2 tons/ac. or 90 lb./1,000 sq. ft. (two to three bales). The mulch shall be spread uniformly by hand or mechanically so the soil surface is covered
- or uniform distribution of hand-spread mulch, divide area into approximately 1,000 sq. ft. sections and spread two 45-lb. bales of straw in each section. Hydroseeders--If wood cellulose fiber is used, it shall be used at 2,000 lb./ac. or 46 lb./1,000 sq. ft.
- Other--Other acceptable mulches include mulch mattings applied according to manufacturer's recommendations
- or wood chips applied at 6 tons/ac. Straw Mulch Anchoring Methods
- Straw mulch shall be anchored immediately to minimize loss by wind or water.
- Mechanical--A disk, crimper, or similar type tool shall be set straight to punch or anchor the mulch material into the soil. Straw mechanically anchored shall not be finely chopped by, generally, be left longer than 6 in.

PERMANENT SEEDING (1)

- Permanent seeding shall not be considered established for at least 1 full yr. from the time of planting. Seeded areas shall be inspected for failure and vegetation conditions, it may be necessary to irrigate, fertilize, overseed, or reestablish plantings in order to provider permanent vegetation for adequate erosion control.
- Maintenance fertilization rates shall be established by soil test recommendations or by using the rates shown in the following table. DORMANT SEEDINGS.
- Seeding shall not be planted from October 1 through November 20. During this period the seeds are likely to germinate but probably will not be able to survive the winter.
- 2. The following methods may be used for "Dormant Seeding":
- * From October 1 through November 20, prepare the seedbed, add the required amounts of lime and fertilizer, then mulch and anchor. After November 20, and before March 15, broadcast the selected seed mixture. Increase the seeding rates by 50%
- * From November 20 through March 15, when soil conditions permit, prepare the seedbed, lime and fertilize, apply the selected seed mixture, mulch and anchor. Increase the seeding rates by 50% for this type of seeding.
- Apply seed uniformly with a cyclone seeder, drill, cultipacker seeder, or hydro-seeder (slurry may include seed and fertilizer) on a firm, moist seedbed.
- Where feasible, except when a cultipacker type seeder is used, the seedbed should be firmed following seeding operations with a cultipacker, roller, or light drag. On sloping land, seeding operations should be on the contour where feasible.
- Mulch Nettings--Nettings shall be used according to the manufacturer's recommendations. Netting may be necessary to hold mulch in place in areas of concentrated runoff and on critical slopes.
- Asphalt Emulsion--Asphalt shall be applied as recommended by the manufacturer or at the rate of 160 gal./ac.
- Synthetic Binders--Synthetic binders such as Acrylic DLR (Agri-Tac), DAC-70, Petroset, Terra Tack or equal may be used at rates recommended by the
- Wood Cellulose Fiber--Wood cellulose fiber binder shall be applied at a net dry weight of 750 lb./ac. The wood cellulose fiber shall be mixed with water and the mixture shall contain a maximum of 50 lb./100 gal. of wood cellulose fiber.
- IRRIGATION
- 1. Permanent seeding shall include irrigation to establish vegetation during dry or hot weather or on adverse site conditions as needed for adequate moisture for seed germination and plant growth.
- 2. Excessive irrigation rates shall be avoided and irrigation monitored to prevent erosion and damage from runoff.

TEMPORARY SEEDING

Temporary seeding provides erosion control on areas in between construction operations. Grasses which are quick growing are seeded and usually mulched to provide prompt, temporary soil stabilization. It effectively minimizes the area of a construction-site prone to erosion and should be used everywhere the sequence of construction operations allows vegetation to be established.

CONDITIONS WHERE PRACTICE APPLIES Temporary seeding should be applied on exposed soil where additional work (grading,etc.) is not scheduled for more than 45 days. Permanent seeding should be applied if the areas will be idle for more than a year.

PLANNING CONSIDERATIONS This practice has the potential to drastically reduce the amount of sediment eroded from a construction-site. Control efficiencies greater than 90% will be achieved with proper applications of temporary seeding. Because practices used to trap sediment are usually much less effective, temporary seeding is to be used even on areas where runoff is treated by sediment trapping practices. Because temporary seeding is highly

Temporary Seeding Spe	cies Selection		
Seeding Dates	Species	Lb./1,000 ft. ²	Per Acre
March 1 to August 15	Oats	3	4 bushel
	Tall Fescue	1	40 lb.
	Annual Ryegrass	1	40 lb.
	Perennial Ryegrass	1	40 lb.
	Tall Fescue	1	40 lb.
	Annual Ryegrass	1	40 lb.
August 16 to November 1	Rye	3	2 bushel
	Tall Fescue	1	40 lb.
	Annual Ryegrass	1	40 lb.
	Wheat	3	2 bushel
	Tall Fescue	1	40 lb.
	Annual Ryegrass	1	40 lb.
	Perennial Ryegrass	1	40 lb.
	Tall Fescue	1	40 lb.
	Annual Ryegrass	1	40 lb.
November 1 to Spring Seeding	Use mulch only, sodd	ing practices or dormant	seeding.

Structural erosion- and sediment-control practices such as diversions and sediment traps shall be installed and stabilized with temporary seeding prior to grading the rest of the construction-site. Temporary seed shall be applied between construction operations on soil that will not be graded or

Note: Other approved seed species may be substituted.

or shall be seeded within 7 days. Several applications of temporary seeing are necessary on typical The seedbed should be pulverized and loose to ensure the success of establishing vegetation.

reworked for 45 days or more. These idle areas should be seeded as soon as possible after grading

vegetation which may require the use of soil amendments. Soil tests should be taken on the site to

- However, temporary seeding shall not be postponed if ideal seedbed preparation is not possible. Soil Amendments--Applications of temporary vegetation shall establish adequate stands of
- Seeding Method--Seed shall be applied uniformly with a cyclone seeder, drill cultipacker seeder, or hydroseeder. When feasible, seed that has been broadcast shall be covered by raking or dragging and then lightly tamped into place using a roller or cultipacker. If hydroseeding is used, the seed and fertilizer will be mixed on-site and the seeding shall be done immediately and without interruption.

MULCHING TEMPORARY SEEDING

Applications of temporary seeding shall include mulch which shall be applied during or immediately after seeding. Seedings made during optimum seeding dates and with favorable soil conditions and on very flat areas may not need mulch to achieve adequate stabilization.

Straw--If straw is used, it shall be unrotted small-grain straw applied at the rate of 2 tons/ac. or 90 lb./1,000 sq. ft. (two to three bales). The mulch shall be spread uniformly by mulch, divide area into approximately 1,000-sq.-ft. sections and spread two 45-lb. bales of

Hydroseeders--If wood cellulose fiber is used, it shall be used at 2,000 lb/ac. or 46

- * Other--Other acceptance mulches include mulch mattings applied according to
- manufacturer's recommendations or wood chips applied at 6 tons/ac.
- 3. Straw mulch shall be anchored immediately to minimize loss by wind or water. Anchoring Methods:
 - the mulch material into the soil. Straw mechanically anchored shall not be finely chopped but, generally, be left longer than 6 in. Mulch Nettings--Nettings shall be used according to the manufacturer's recommendations. Netting may be necessary to hold mulch in place in areas of concentration runoff and on

Mechanical--A disk, crimper, or similar type tool shall be set straight to punch or anchor

- * Asphalt Emulsion--Asphalt shall be applied as recommended by the manufacturer or at the rate of 160 gal./ac.
- * Synthetic Binders--Synthetic binders such as Acrylic DLR (Agri-Tac), DCA-70, Petroset, Terra Tack or equal may be used at rates recommended by the manufacturer * Wood-Cellulose Fiber--Wood-cellulose fiber binder shall be applied at a net dry weight of 750 lb./ac. The wood-cellulose fiber shall be mixed with water and the mixture shall contain a maximum of 50 lb./100 gal.

CONSTRUCTION ENTRANCE Stone Size--2 to 3 inch stone shall be used, or recycled concrete

- equivalent. Length--The construction entrance shall be as long as required to stabilize high traffic areas but not less than 50 ft. (except on single
- residence lot where a 30-ft. minimum length applies). 3. Thickness--The stone layer shall be at least 6 to 12 inch thick.
- Width--The entrance shall be at least 20 ft. wide. Busy entrances will need the capacity of handling a lane of traffic each way, typically 30 feet wide flare the entrance where it meets the existing
- Bedding--If soft soil condition exist, place a subgrade stabilization fabric over the entire length and width of the entrance prior to placing the rock.

road to provide a turning raduis.

6. Culvert--A pipe or culvert shall be constructed under the entrance if needed to prevent surface water flowing across the entrance from being directed out onto paved surfaces.

Water Bar--A water bar shall be constructed as part of the

construction entrance if needed to prevent surface runoff from

flowing the length of the construction entrance and out onto paved

- 8. Maintenance--Top dressing of additional stone shall be applied as conditions demand. Mud spilled, dropped, washed or tracked onto public roads, or any surfaces where runoff is not checked by sediment controls, shall be removed immediately. Removal shall
- Construction entrances shall not be relied upon to remove mud fro vehicles and prevent off-site tracking. Vehicles that enter and leave the construction-site shall be restricted from muddy areas.

be accomplished by scraping or sweeping.

CITY/STATE:

DANDY BAG DETAIL

18" or Sufficient

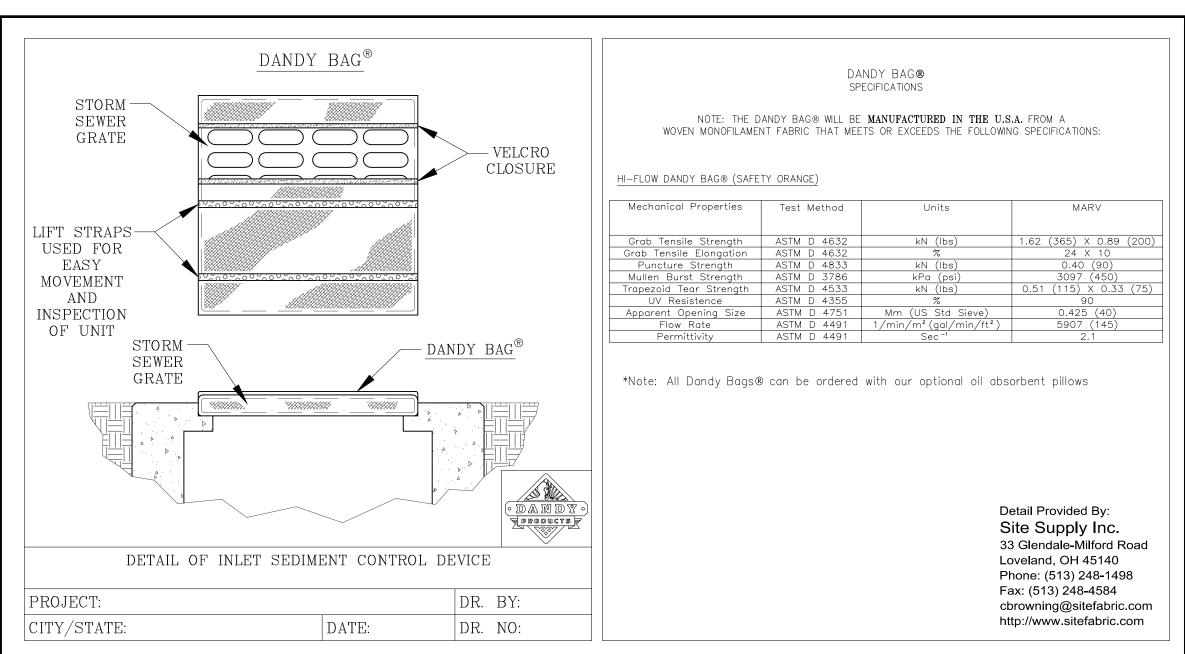
to Divert Runoff

50 ft. (or 30' for Acess to Individual House

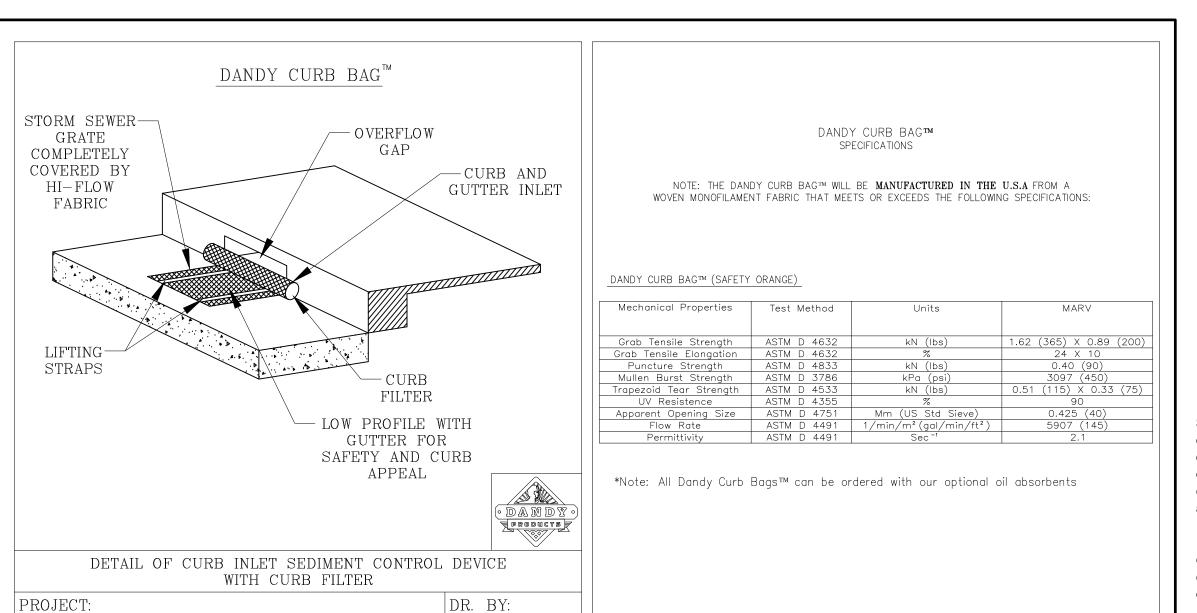
R/W Diversion

as Needed

Existing Paved Surface ~



DANDY CURB BAG DETAIL



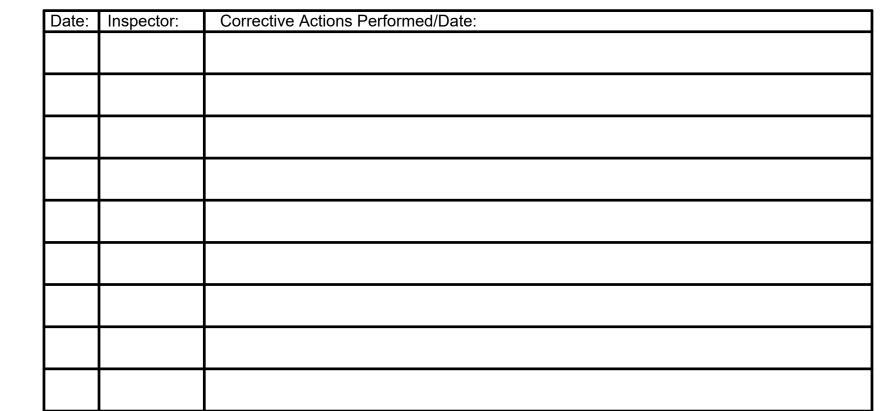
Erosion Prevention and Sediment Control Site Inspection Form hand or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. For uniform distribution of hand-spread land or mechanically so the soil surface is covered. when the action was completed.

DR. NO:

Inspection Log

DATE:

The site shall be inspected before and after storm events with 0.5 inches or greater predicted or actual precipitation, and documented on the Construction Site Inspection Form. Incidents of noncompliance must be reported to the Engineer. A log of all inspections, as shown below, shall be kept current.



1. Silt fence shall be constructed before upslope land disturbance begins.

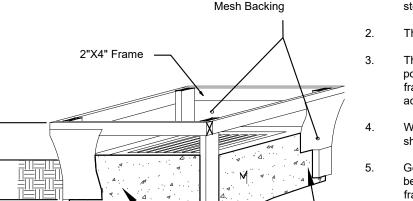
ELEVATION

- Flat Slope in Front of Barrier -

- All silt fence shall be placed as close to the contour as possible so that water will not concentrate at low points in the fence and so that small swales or depressions which may carry small concentrated flows to the silt fence are dissipated along its length.
- To prevent water ponded by the silt fence from flowing around the ends, each end shall be
- constructed upslope so that the ends are at a higher elevation. Where possible, silt fence shall be placed on the flattest area available.
- Where possible, vegetation shall be preserved for 5 ft. (or as much as possible) upslope from the silt fence. If vegetation is removed, it shall be reestablished within 7 days from the installation of the silt fence.
- 6. The height of the silt fence shall be a minimum of 16 in. above the original ground surface.
- The silt fence shall be placed in a trench cut a minimum of 6 in, deep. The trench shall be cut with a trencher, cable laying machine, or other suitable device which will ensure an
- adequately uniform trench depth. The silt fence shall be placed with the stakes on the downslope side of the geotextile and so that 8 in. of cloth are below the ground surface. Excess material shall lay on the bottom of the 6-in.-deep trench. The trench shall be backfilled and compacted.
- Seams between section of silt fence shall be overlapped with the end stakes of each section wrapped together before driving into the ground.
- Maintenance--Silt fence shall allow runoff to pass only as diffuse flow through the geotextile. If runoff overtops the silt fence, flows under or around the ends, or in any other way becomes a concentrated flow, one of the following shall be performed, as appropriate: 1) The layout of the silt fence shall be changed, 2) Accumulated sediment shall be removed, or 3) Other practices shall be installed.
- Criteria for Silt Fence Materials Fence Posts--The length shall be a minimum of 32 in. long. Wood posts will be 2-by-2-in.
- hardwood of sound quality. The maximum spacing between posts shall be 10 ft. Silt Fence Fabric shall Geotextile Fabric or as described by the chart below:

Geotextile Over Wire

INLET PROTECTION IN SWALES, DITCH LINES OR YARD INLETS



Around Inlet

- Inlet protection shall be constructed either before upslope land disturbance begins or before the storm drain becomes operational.
- The earth around the inlet shall be excavated completely to the depth at least 18in. The wooden frame shall be constructed of 2-by-4-in. construction grade lumber. The 2-by-4-in. posts shall be driven 1 ft. into the ground at four corners of the inlet and the top portion of 2-by-4-in. frame assembled using the overlap joint shown. The top of the frame shall be at least 6 in. below

inlet so the ends of the cloth are not fastened to the same post.

- adjacent roads if ponded water would pose a safety hazard to traffic. Wire mesh shall be of sufficient strength to support fabric with water fully impounded against it. It
- shall be stretched tightly around the frame and fastened securely to the frame. Geotextile shall have an equivalent opening size of 20-40 sieve and be resistant to sunlight. It shall be stretched tightly around the frame and fastened securely. It shall extend from the top of the frame to 18 in. below the inlet notch elevation. The geotextile shall overlap across one side of the
- Backfill shall be placed around the inlet in compacted 6-in. layers until the earth is even with notch elevation on ends and top elevation on sides.
- A compacted earth dike or a check dam shall be constructed in the ditch line below the inlet if the inlet is not in a depression and if runoff bypassing the inlet will not flow to a setting pond. The top of earth dikes shall be at least 6 in. higher than the top of the frame.

Erosion Prevention and Sediment Control Site Inspection Form

Introduction: By using some simple Best Management Practices (BMP's) **developers and** contractors can do their share to protect the County's water resources from the harmful effects of sediment. The topography of the site and the extent of the construction activities will determine which of these practices are applicable to any given site, but the BMP's listed here are applicable to most construction sites. For details on the installation and maintenance of Site Erosion Control Manual.

Temporary Stabilization is the most effective BMP. All disturbed areas that will lie dormant for 21 days or more must be stabilized within 7 days of the date the area becomes inactive. The goal of temporary stabilization is to provide cover quickly. Areas within 50 feet of a stream must be stabilized within 2 days of reaching final grade. This is accomplished by seeding with fast-growing grasses, then covering with straw mulch. See the Rainwater and Land Development Manual for seasonally adjusted seeding specifications. To Action Needed: minimize your costs of temporary stabilization, leave natural cover in place for as long as possible by only disturbing areas worked within the next 21 days.

Construction Entrances are installed to minimize off-site tracking of sediments. A roughstone access drive underlain with woven geotextile shall be installed at every point where vehicles enter or exit the site. Every individual lot should also have its own drive once construction on the lot begins. Maintenance is performed by top dressing with stone and/or street sweeping.

Sediment Basins/Traps are the sediment control of choice for areas, which exceed the design capacity of silt fence (see page 119 of the Rainwater manual) or to control concentrated flows or runoff. There are two types: sediment basins and sediment traps. A trap is appropriate where the contributing drainage area is 10 acres or less. The outlet is an earthen embankment with a simple stone spillway underlain with woven geotextile. A sediment basin is appropriate for drainage areas larger than 10 acres. The

outlet is an engineered riser pipe. Often a permanent storm water management pond, such as a retention or detention basin, can be retrofitted to whether they are a trap or a basin, or whether they will become a permanent storm water pond, must provide a minimum storage of 67 cubic yards per acre of total contributing drainage area. Sediment ponds must be installed prior to mass clearing and grading. Maintenance must be performed once the basin loses 40% of capacity, and 30% for storm water basins retrofitted as sediment

Silt Fence or Mulch Berms are typically used at the perimeter of a ditch, pipes or though streams. For large drainage areas where flow is concentrated, collect runoff in diversion berms or channels and pass it through a sediment pond prior to discharging it from the site. Combination barriers

Erosion Prevention and Sediment Control Site Inspection Form

Amount of rainfall since last inspection: _____ Overall site conditions: **Construction Entrances:** these BMP's, please refer to the approved plans and or the Iowa Construction Is the entrance installed correctly according to the approved plan? YES NO N/A (Check for mud in stones/street, runoff diverted from street, etc..)

Sediment Basins/Traps:

Action Needed:

Are all Basins installed correctly according to the approved plan? YES NO N/A (Check for runoff directed to basin, down slope areas stabilized, riser pipe wrapped with wire fence/filter fabric, emergency overflow, accumulated sediment more than 40% of volume, etc..)

Inlet Protection:

Silt Fence/Mulch Berms:

Are all Silt Fence/Mulch Berm (SF/MB) installed correctly according to the approved plan? YES NO N/A (Check for fabric trenched in, follow contour, turned upslope at ends, silt accumulated, broken stakes, tight fabric, installed in all areas where sediment could leave the site) Action Needed:

Are all Inlet Protections installed correctly according to the approved plan? YES NO N/A Check for runoff ponding, in good shape, silt accumulated, etc..)

Action Needed: Temporary Stabilization:

Are all disturbed areas that will lie dormant for 21 days or more stabilized with seed/straw or mulch? (stockpiles, hillsides, etc..) YES NO N/A

act as a sediment basin during construction. All sediment ponds, regardless of Are all areas stabilized still in good condition and not eroding? YES NO N/A

Permanent Stabilization: Have areas that achieved final grade within the last 7 days been stabilized? YES NO N/A

Do all storm water outflow areas have riprap or concrete to prevent scouring? YES NO N/A

disturbed area. They are only for small drainage areas on relatively flat slopes Are the Stream Crossings installed correctly according to the approved plan? YES NO N/A or around small soil storage piles; not suitable where runoff is concentrated in a (Check for stabilized edges, runoff diverted from stream, mud over stones, end of useful life, etc..) Action Needed:

constructed of silt fence supported by welded wire fencing, mulch berms supported by rock check dams, or silt fence embedded within rock check dams may be effective within small channels. As with all sediment controls, silt fence or mulch berms must be capable of ponding runoff so that sediment can settle out of suspension. These must be installed within 7 days of first grubbing the area it controls. Whenever practical they should be installed before clearing or grubbing the area it controls.

protection is still recommended, as it will reduce the amount of sediment entering the basin and increase the overall sediment removal efficiency. Best used on roads with little or no traffic. If working properly, inlet protection will cause water to pond. If used on curb inlets, streets will flood temporarily during heavy storms, (overflow should be built-in.) Check with the authority that has jurisdiction over the roads before installing. They may prefer an alternate BMP. Care should be taken when placing inlet protection so that the runoff is not diverted to public roads or other areas where it could cause a hazard.

Permanent Stabilization must occur on areas at final grade within 7 days of reaching final grade. This is usually accomplished by using seed and mulch, but special measures are

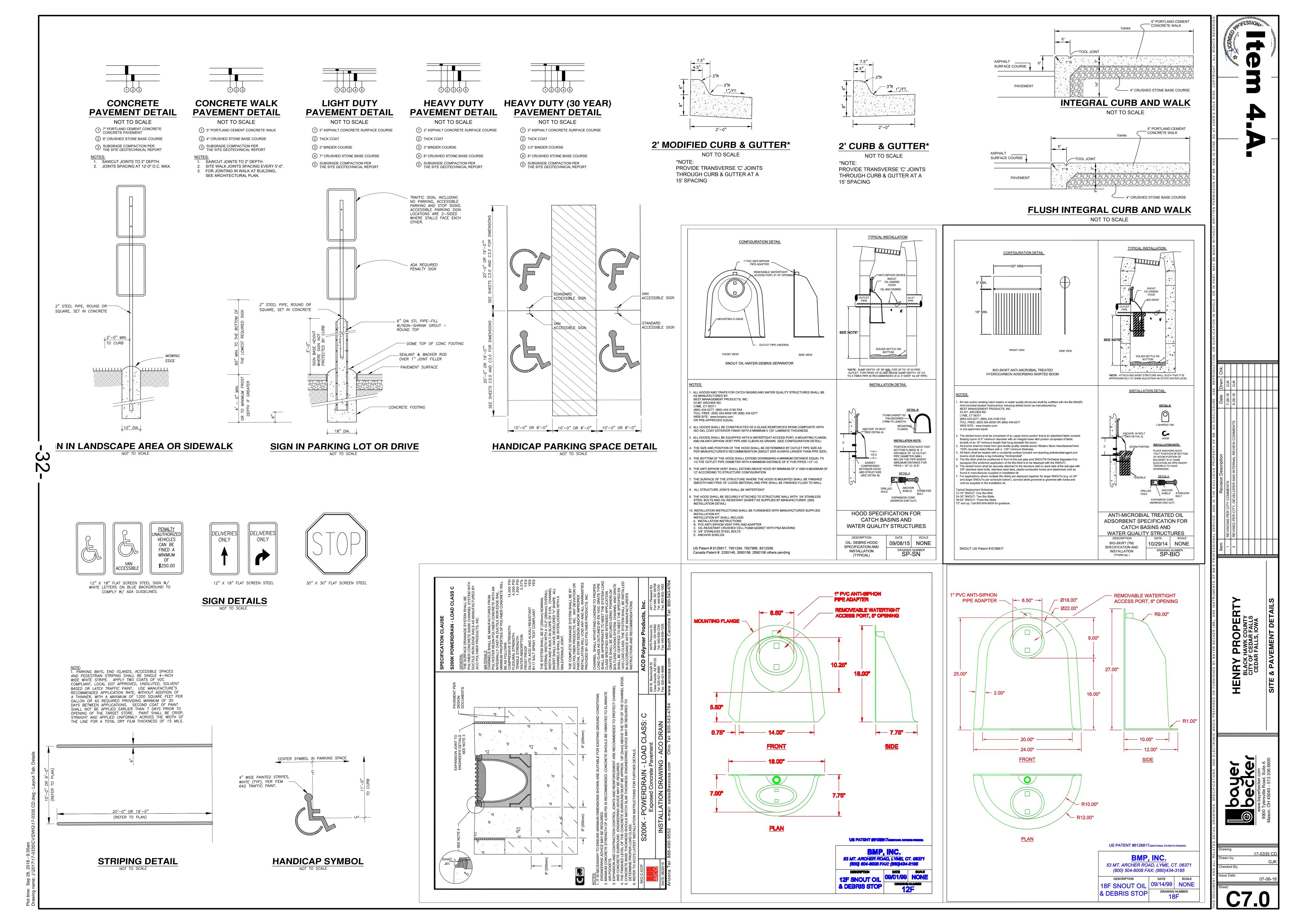
Inlet Protection must be installed on all yard drains and curb drains when these inlets do not drain to a sediment trap or basin. Even if there is a sediment trap or basin, inlet

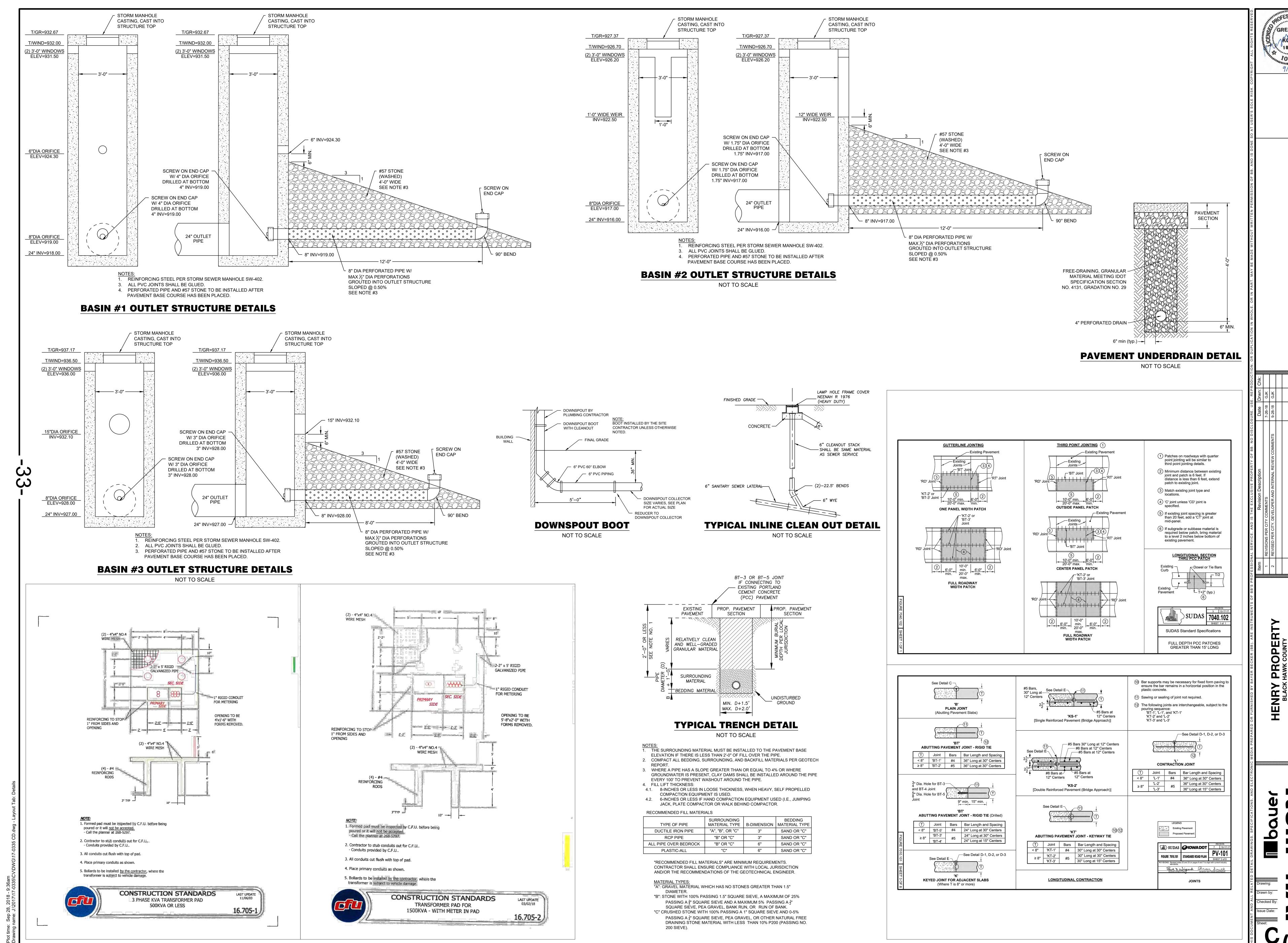
sometimes required. This is particularly true in drainage ditches or on steep slopes. These measures include the addition of topsoil, erosion control matting, rock riprap or retaining walls. See the Rainwater and Land Development Manual for seasonally adjusted seeding specifications. At all times of the year, the area should be temporarily stabilized until a permanent seeding can be applied. Inspections shall be performed at least once a week and within 24 hours after a storm event greater than 1/2 inch of rainfall within a 24-hour duration using the enclosed

Inspection Form. Inspections can be tracked using the enclosed Inspection Log. These shall be maintained throughout the development process and kept on file for three years per OEPA requirements. Erosion prevention and sediment control (EP&SC) measures shall be observed to ensure correct operation. Discharge locations shall be inspected to determine effectiveness of EP&SC measures in preventing significant impacts to the receiving waters. Where practices require repair or maintenance, it must be accomplished within three days of the inspection or as soon as site conditions allow. Repairs to sediment ponds shall be completed within 10 days or as soon as site conditions allow. Most of these BMP's are easy to implement with a little bit of planning and go a long way toward keeping your site clean and organized if they are properly installed and maintained. Please be sure to inform all parties on site how these BMPs affect their operations on the site, particularly those that will be working near a stream.

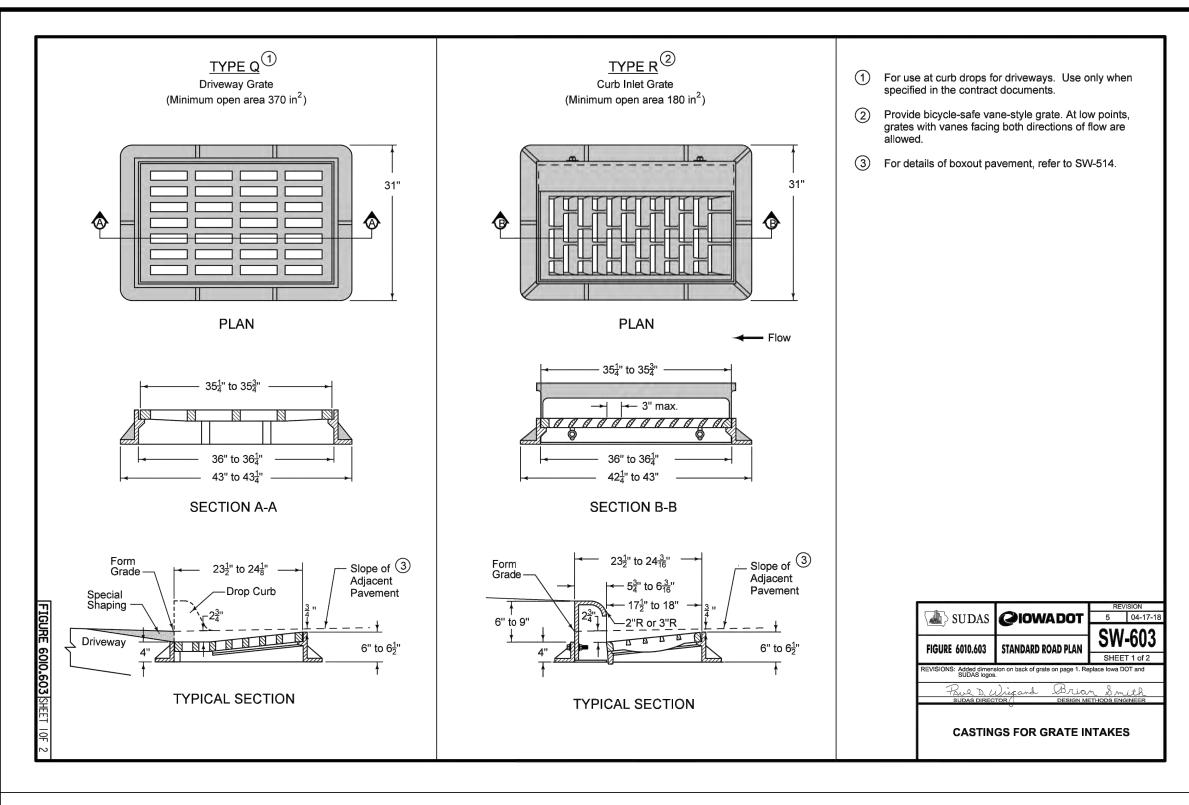


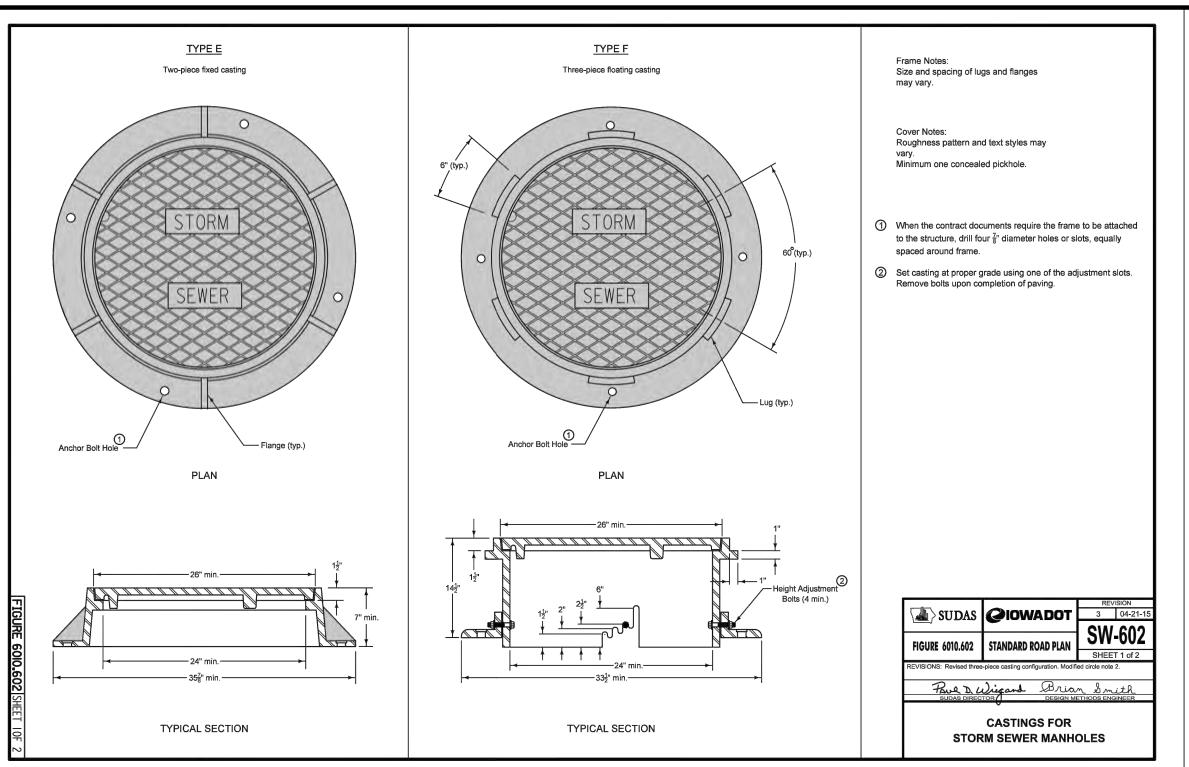
OS

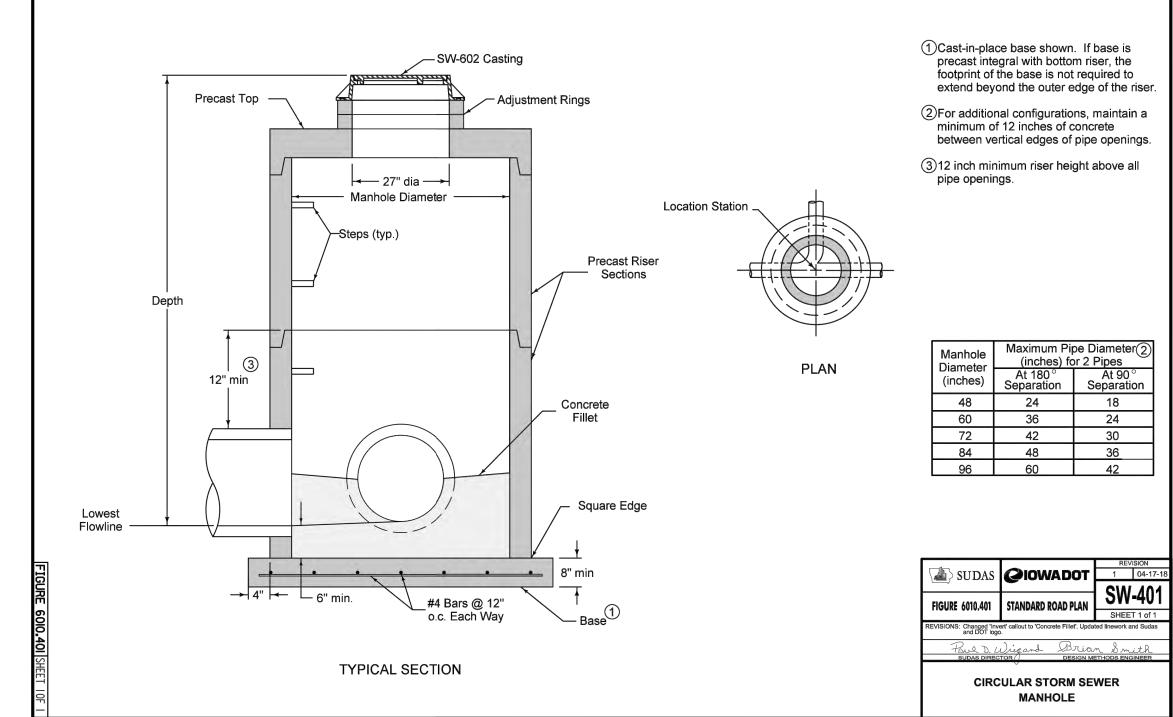


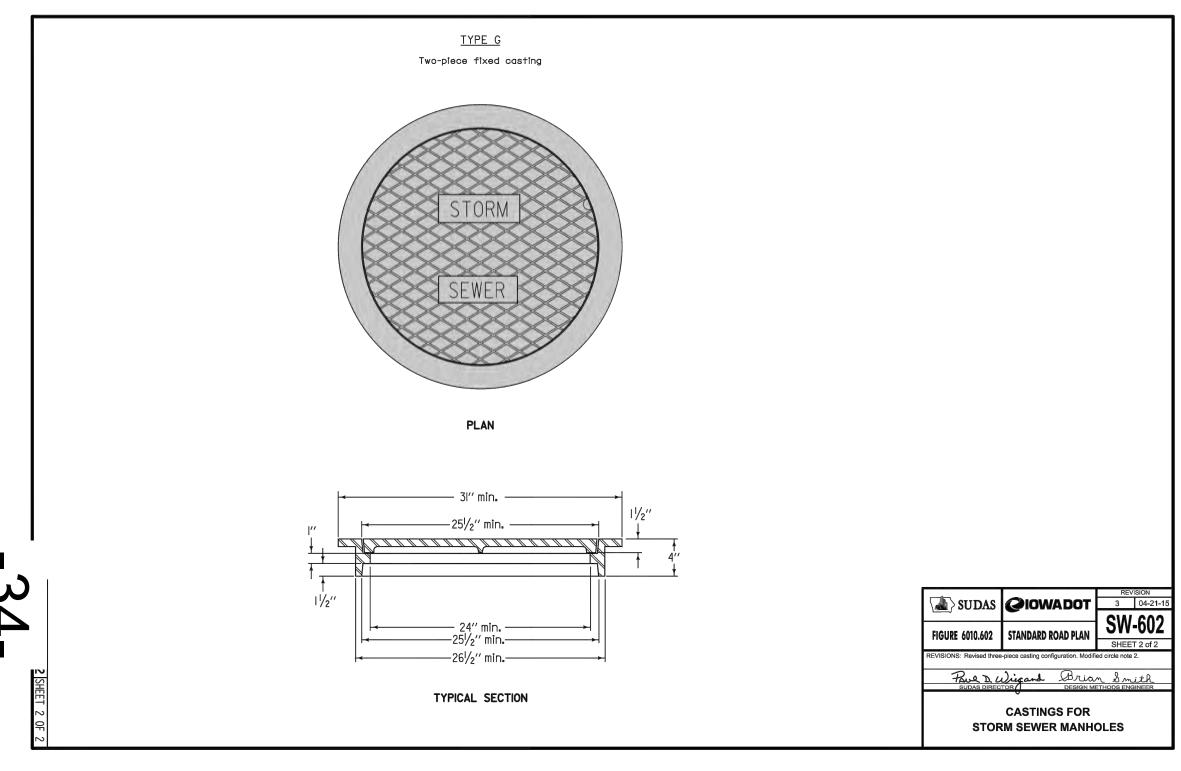


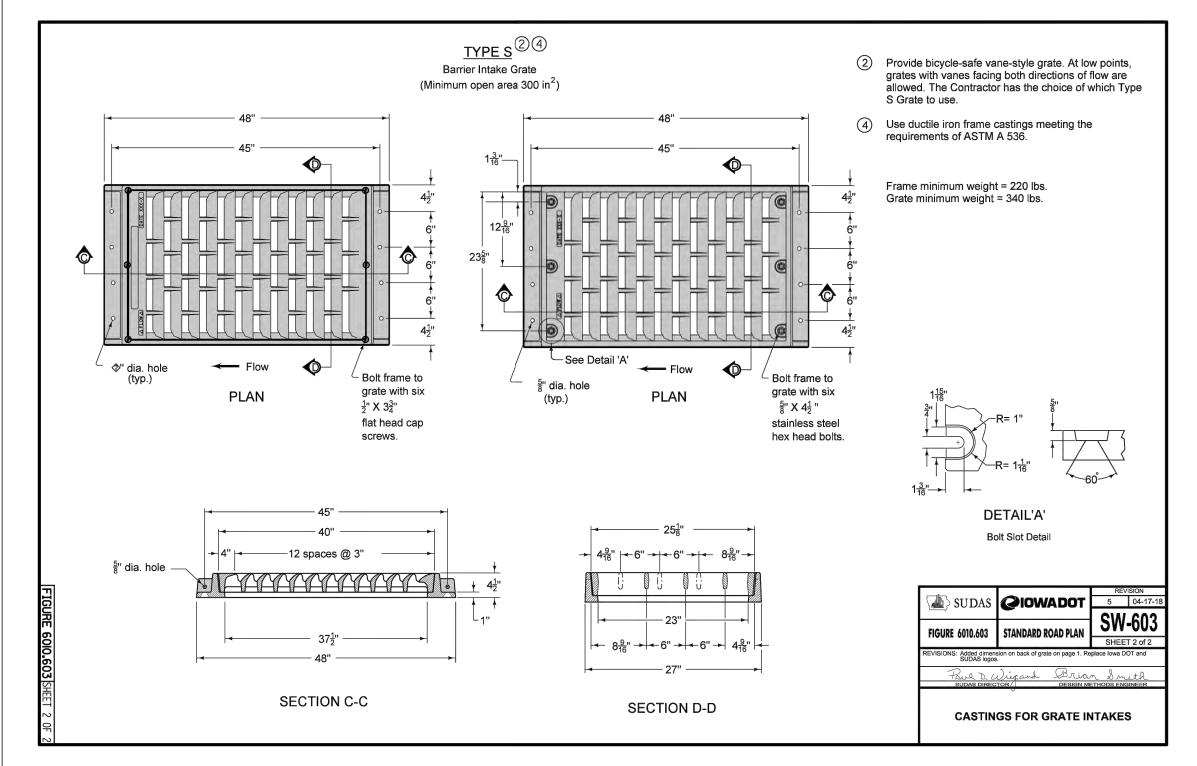
MK CO EDAR F ALLS, IV

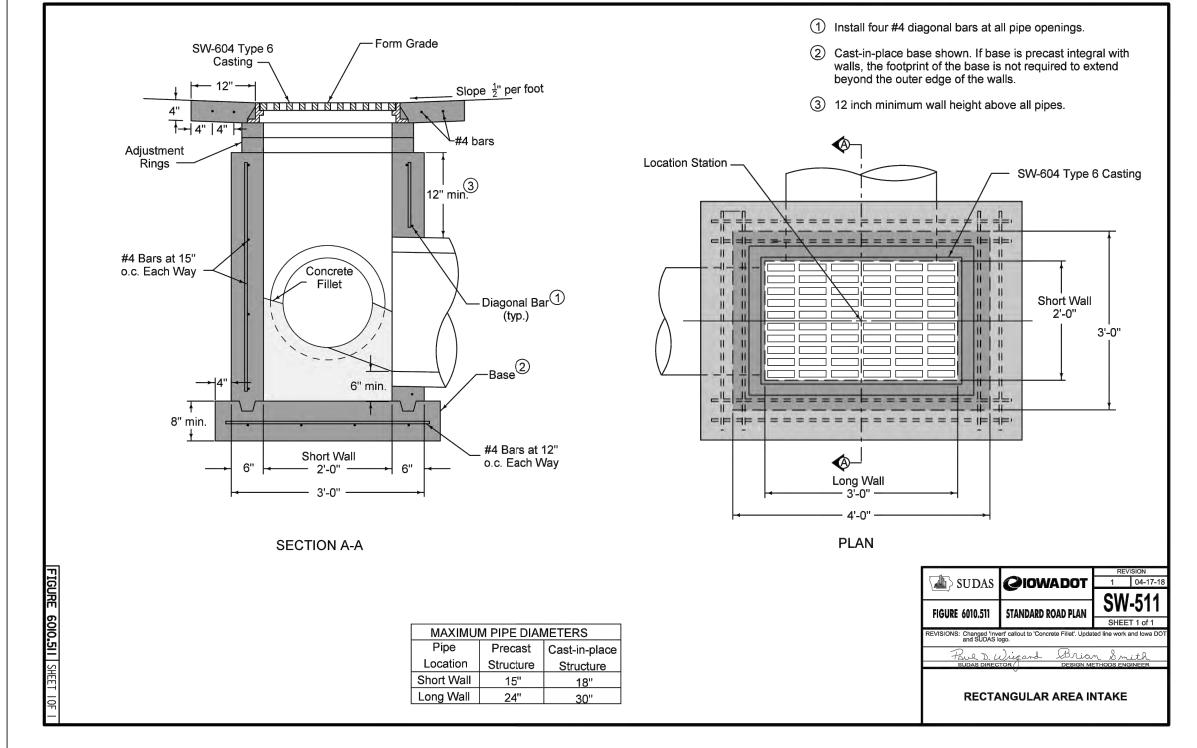


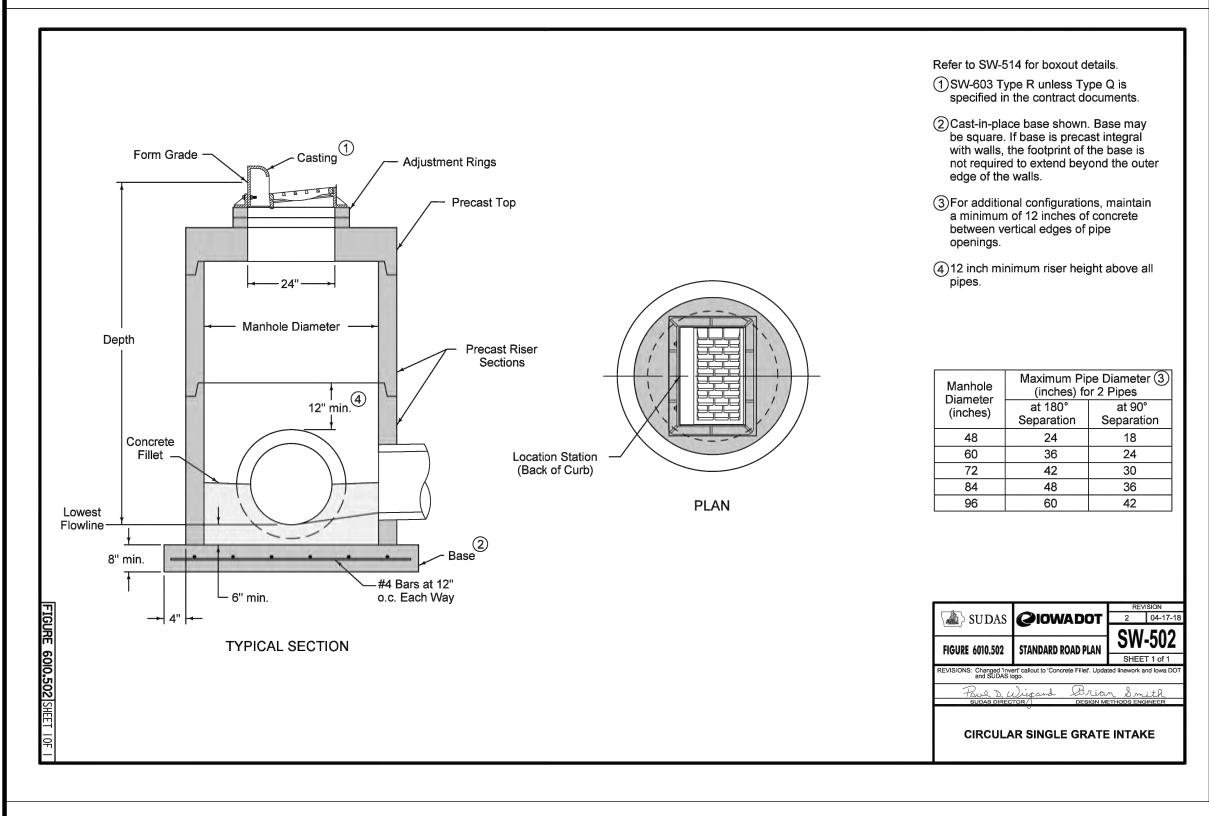


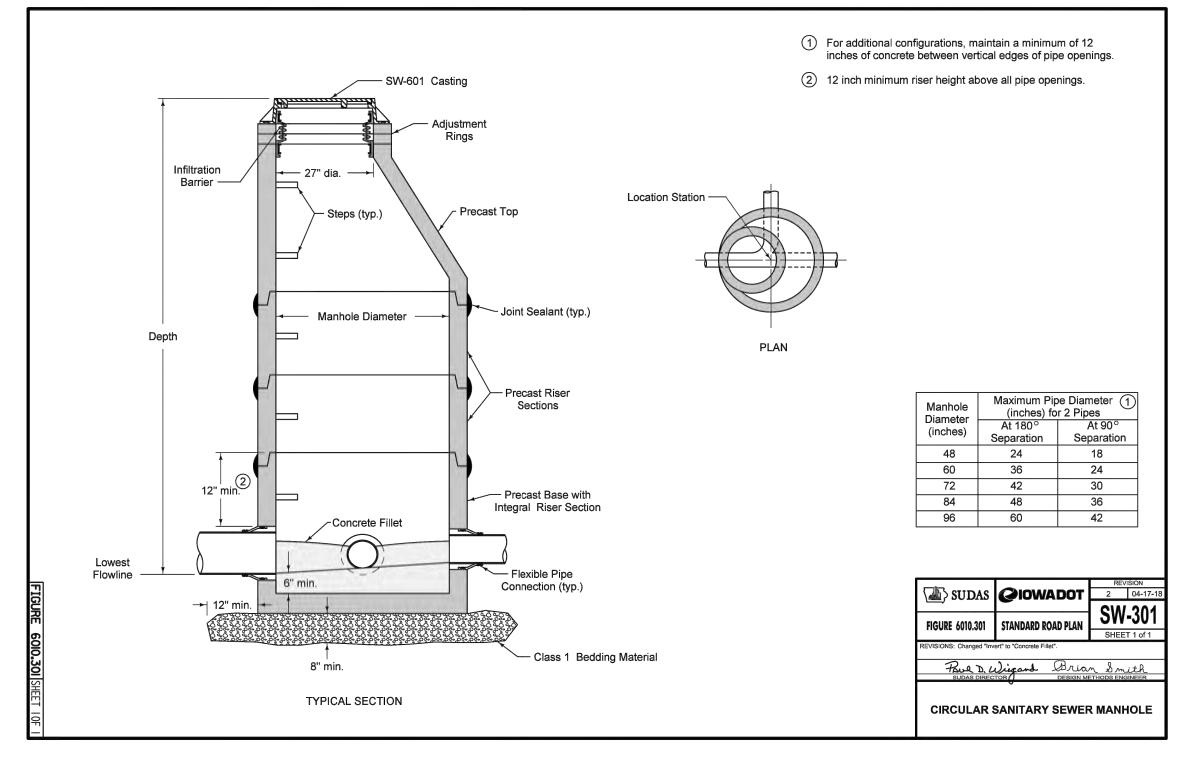


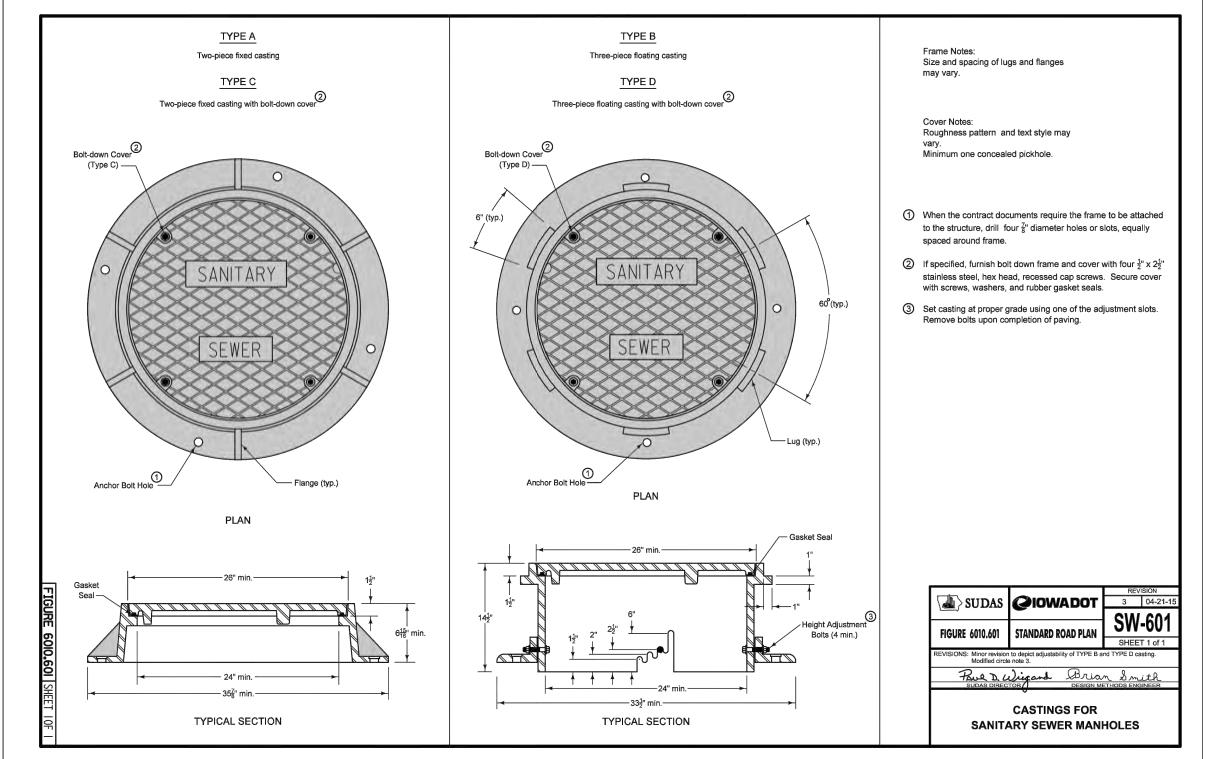












Plot time: Sep 18, 2018 - 2:38pm Drawing name: J:\2017\17-0335\CV\DWG\17-0335 CD.dwg - Lavout Tab: Details DOUGE

 DOUGE

 Www.bayerbecker.com
 6900 Tylersville Road, Suite A
 Mason, OH 45040 - 513.336.6600

ROPE WK COUNTY EDAR FALLS ALLS, IOWA

Drawing:

17-0335 CD

Drawn by:

GJK

Checked By:

C7.2







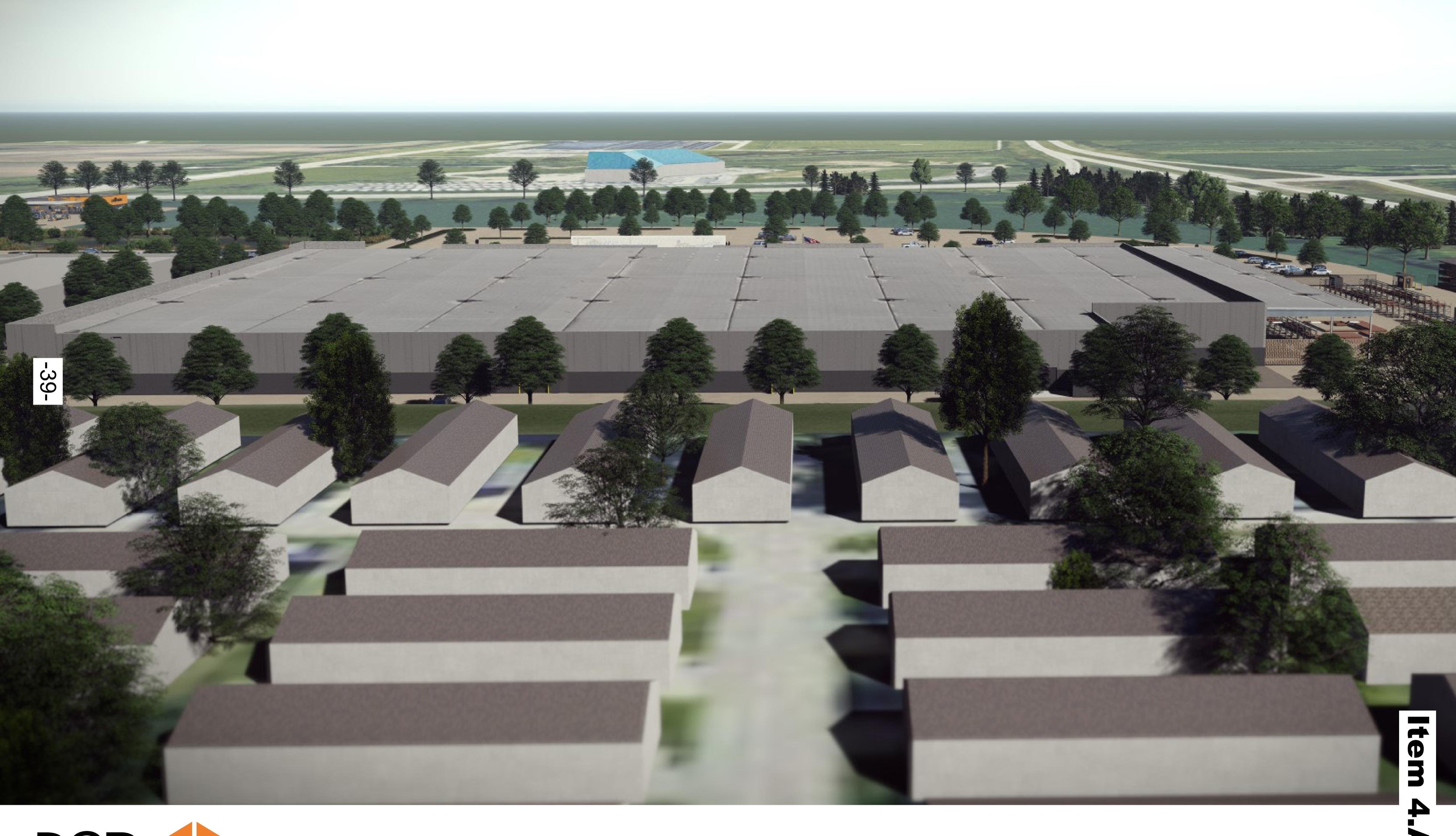






































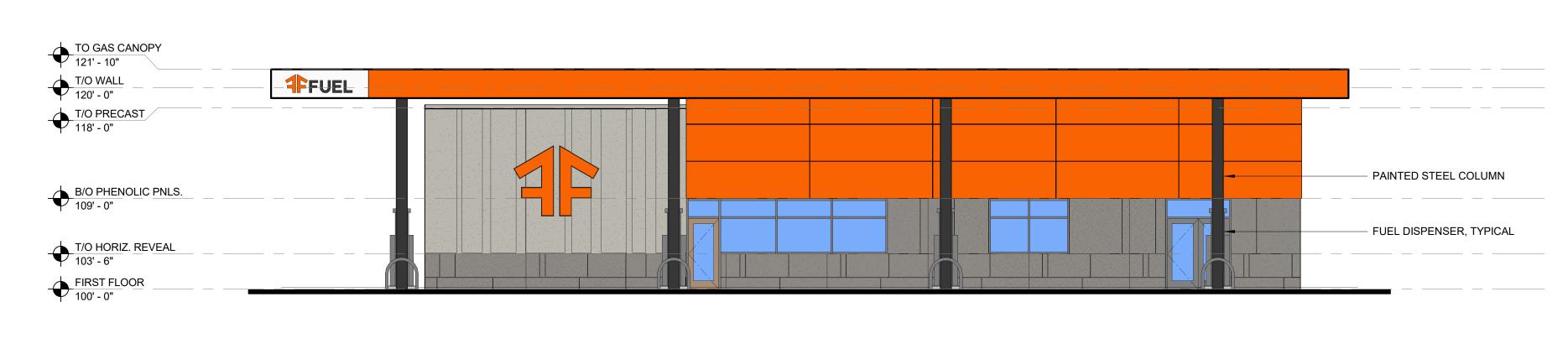






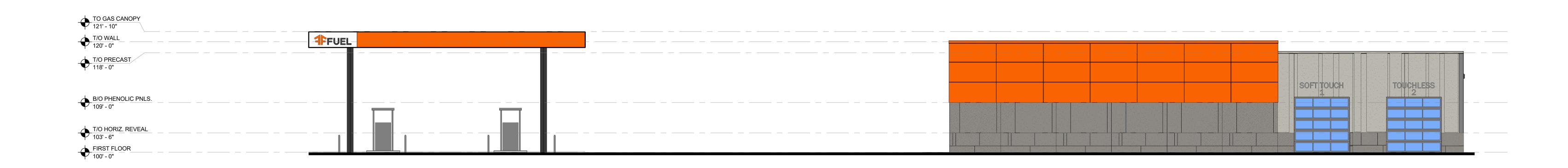
4 LEFT ELEVATION

1/16" = 1'-0"



WEST ELEVATION

1/8" = 1'-0"



NORTH ELEVATION
1/8" = 1'-0"

TO GAS CANOPY
121'-10"

TO WALL
120'-0"

TO PRECAST
118'-0"

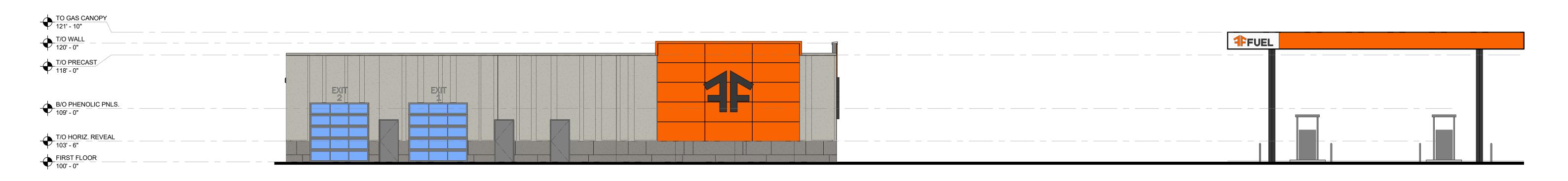
BIO PHENOLIC PNLS.
109'-0"

T/O HORIZ REVEAL
103'-6"

FIRST FLOOR
100'-0"

3 EAST ELEVATION

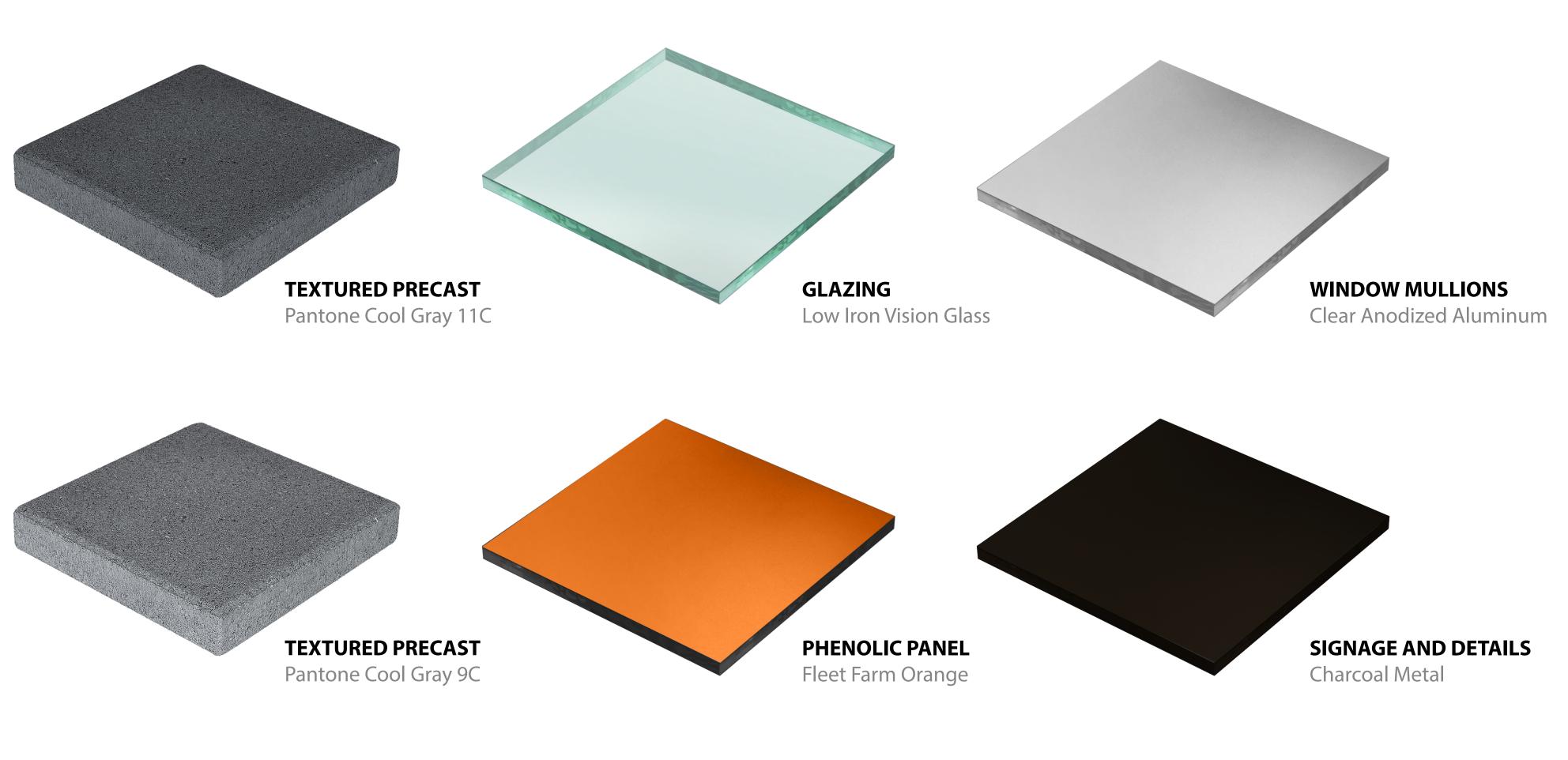
1/8" = 1'-0"



SOUTH ELEVATION

1/8" = 1'-0"















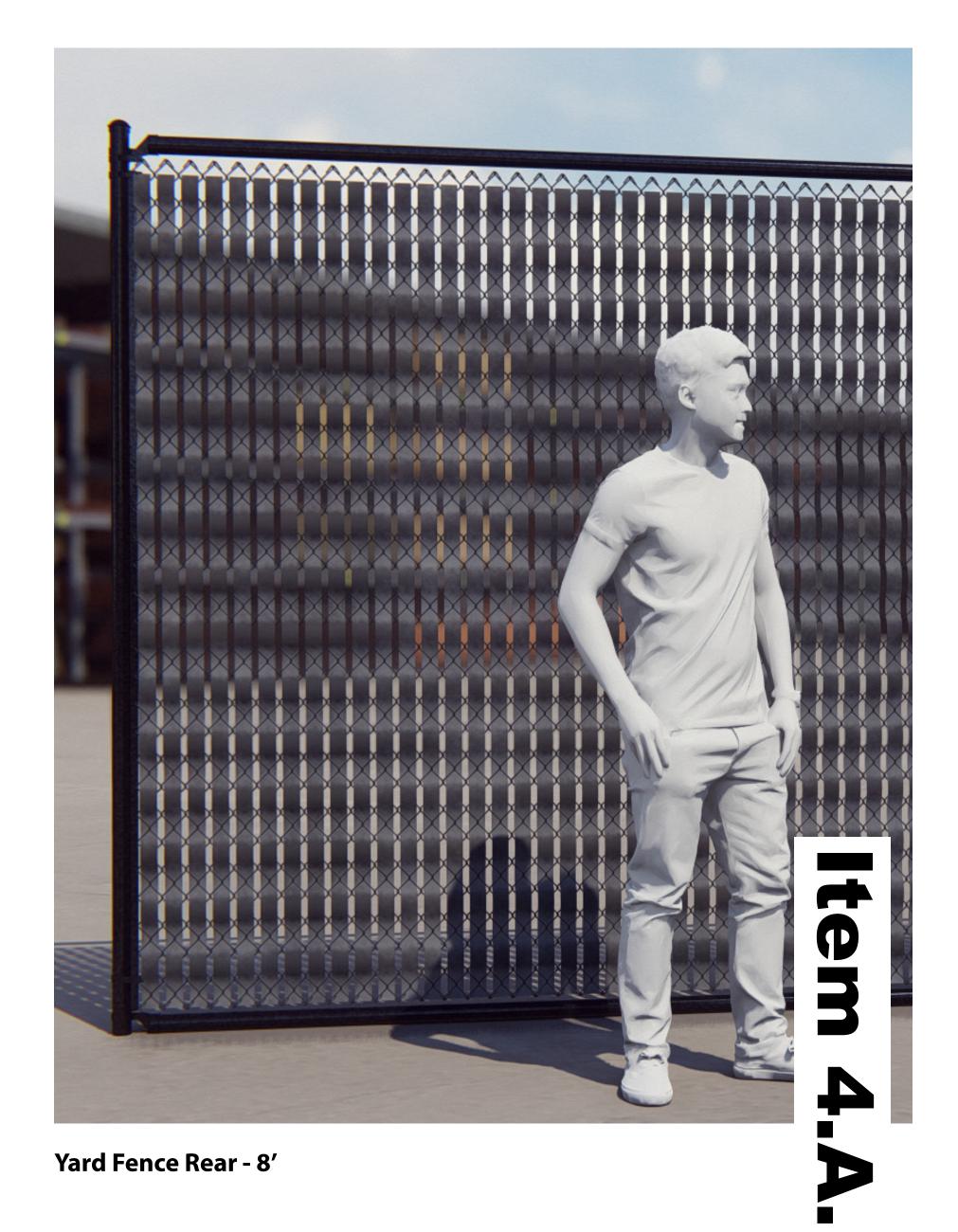


49-









CEDAR FALLS, IA

FLEET FARM -RETAIL STORE

Henry Property

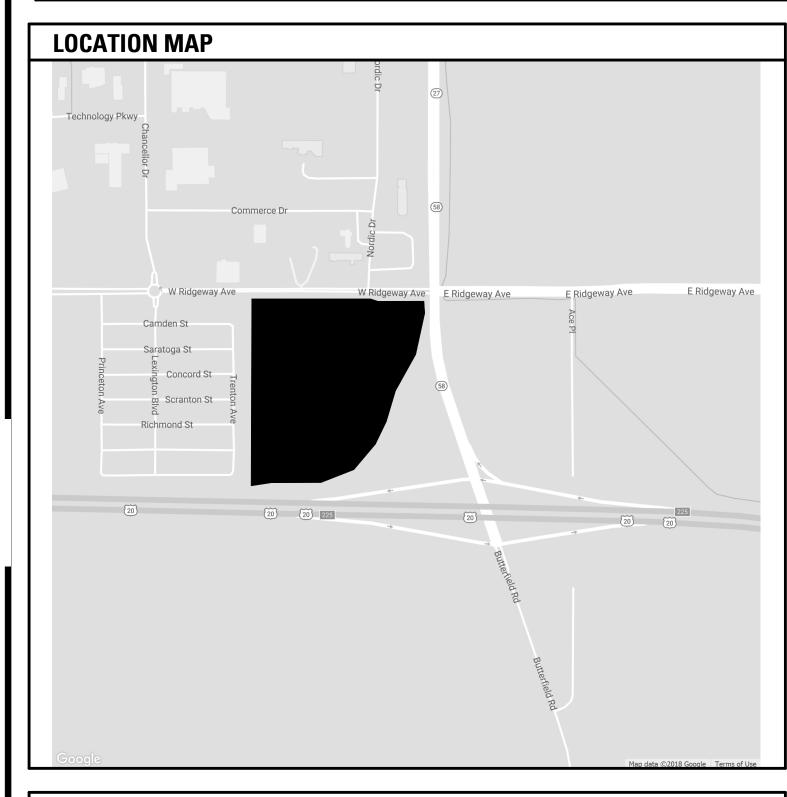
295210700 PROJECT NUMBER:

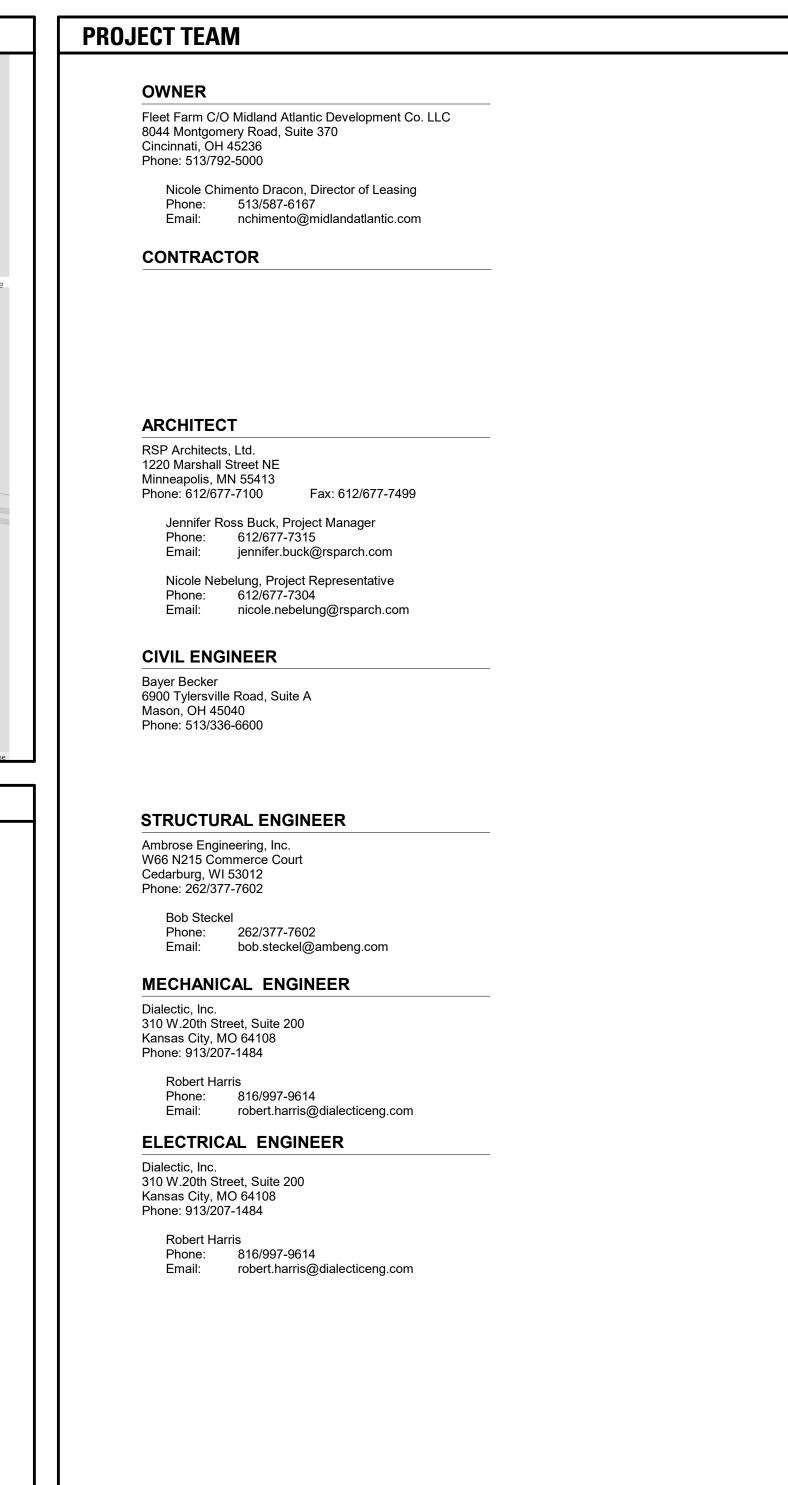




ISSUED: .09.2018 PACKAGE: PLANNING & ZONING SUBMITTAL

DOCUMENT PACKAGE





ELEVATION TAG-

ELEVATION TAG-MULTIPLE VIEW

SECTION TAG

SECTION TAG

ENLARGED PLAN OR DETAIL REFERENCE

BUILDING

A101

SINGLE VIEW

REVISION REFERENCE

PARTITION TAG

101 (ROOM NUMBER) ROOM

150 SF — (ROOM AREA) TAG

PNT1 (WALL FINISH)
VB1 (BASE FINISH)
CPT1 (FLOOR FINISH)
FINISH TAG ALL SURFACES

KEYNOTE

WINDOW TAG

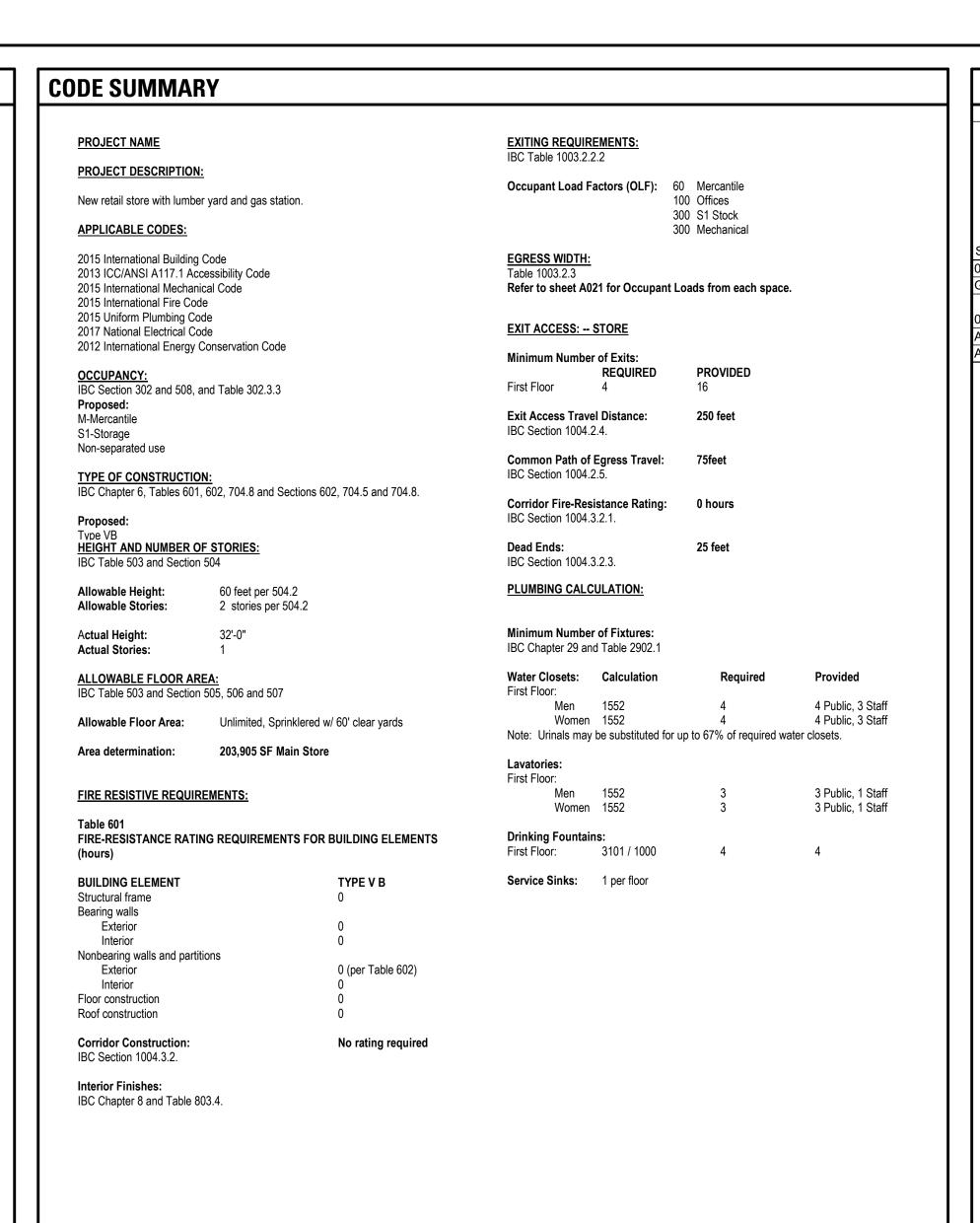
EQUIPMENT TAG

DOOR TAG

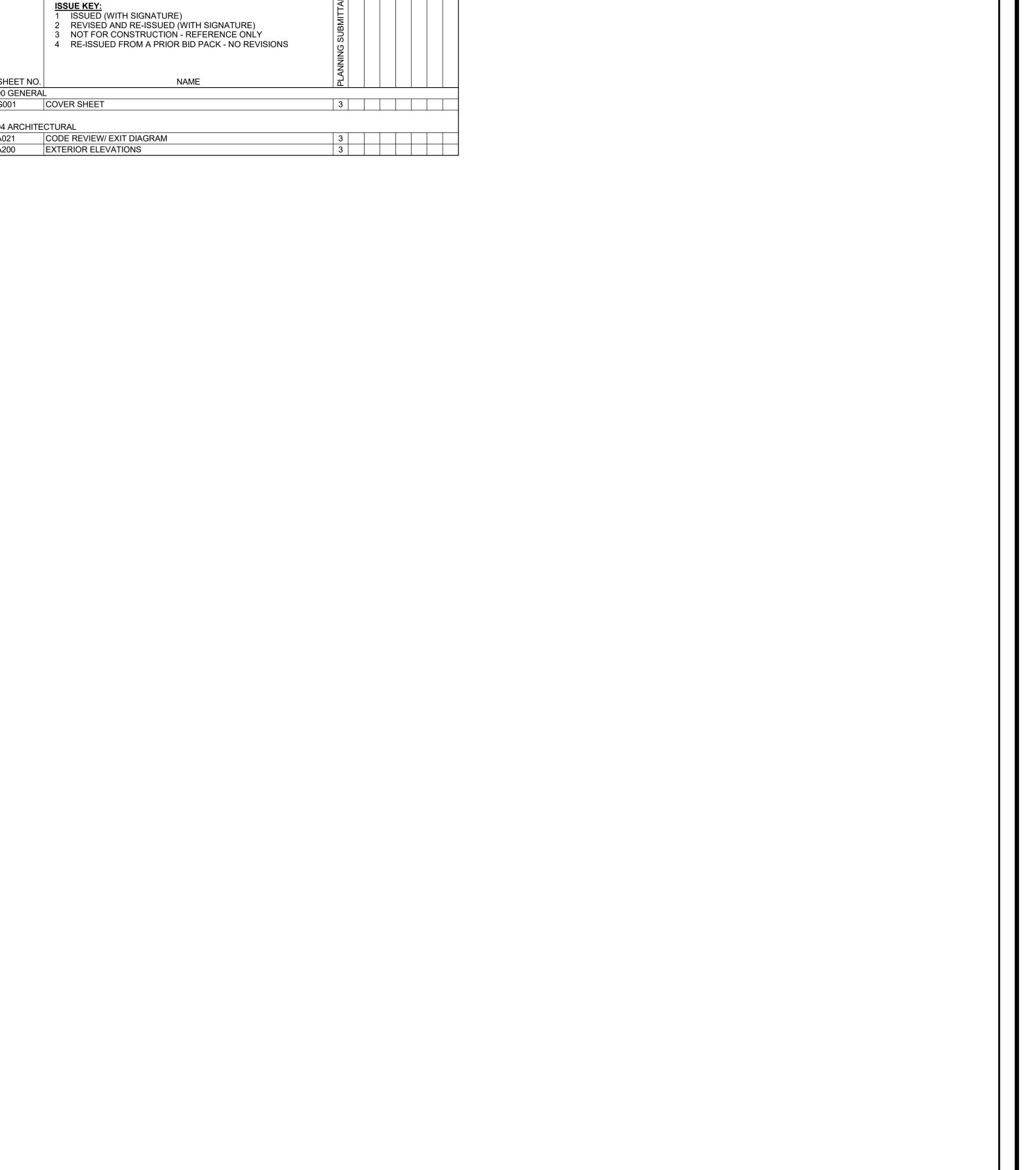
WITH EXTENT

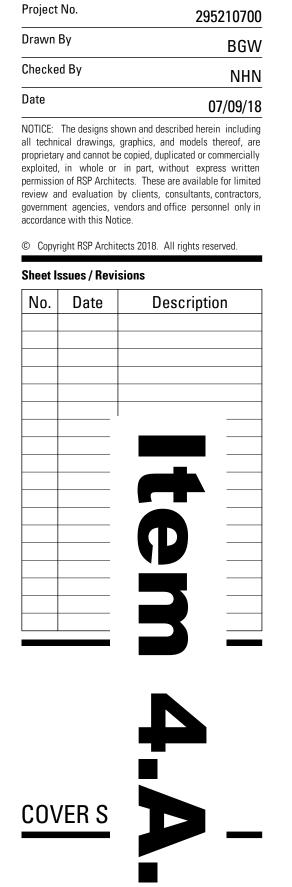
OF FINISH

FINISH TRANSITION



SHEET INDEX





CEDAR FALLS, IA

FLEET FARM -RETAIL

MATCH LINE
SEE X/XXX

DRAWING SYMBOLS

COLUMN OR

GRID LINE - NEW

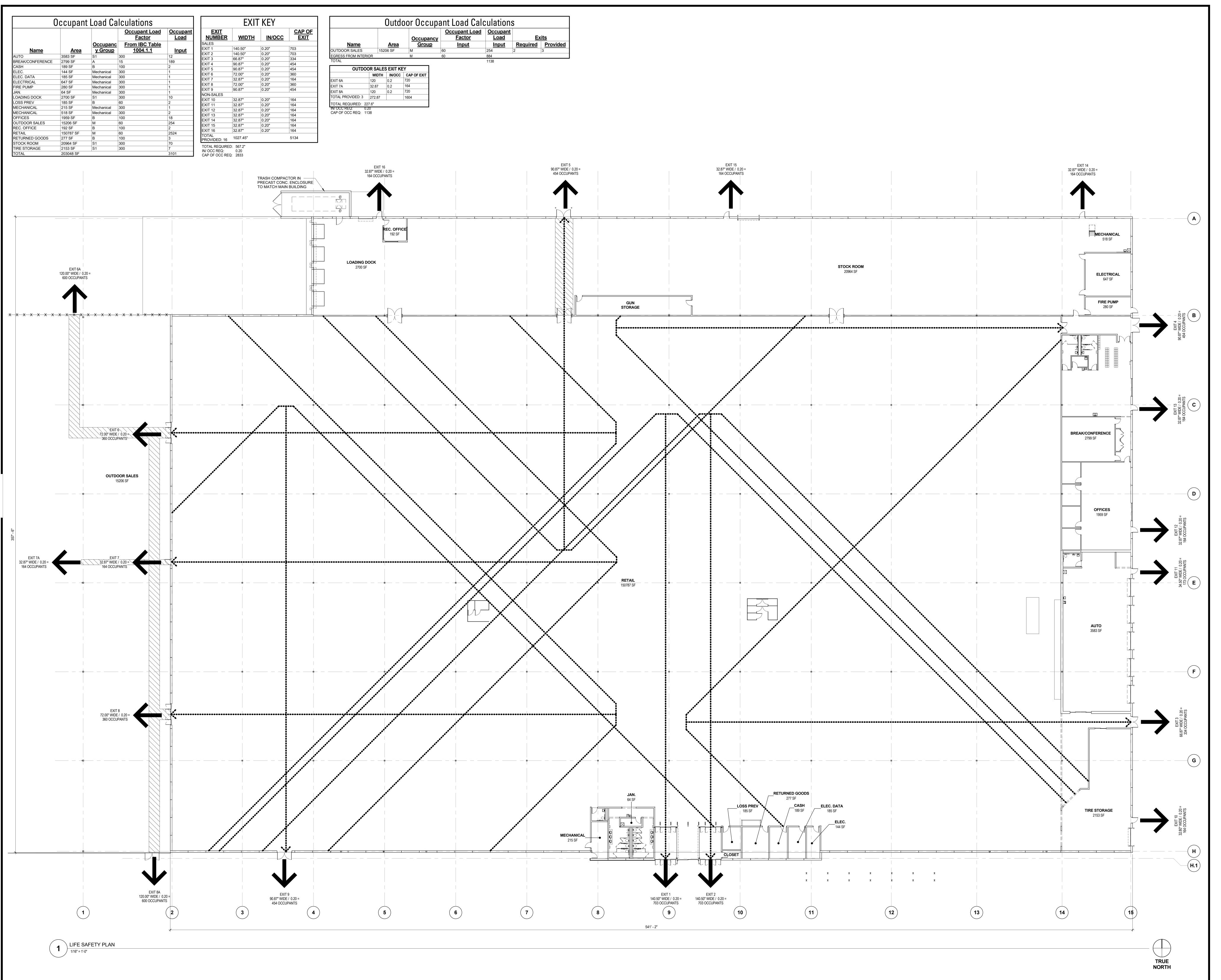
LINE - EXISTING

--- ELEVATION /

LEVEL TAG

SPOT ELEVATION

SITE PLAN



C:\Revit Projects\A17_FF_Cedar Falls_04_BWilson.rvt 7/6/2018 3:50:47 PM





Name
Robert Lucius
License Number
3570
Date Signed

Project For

CEDAR FALLS, IA

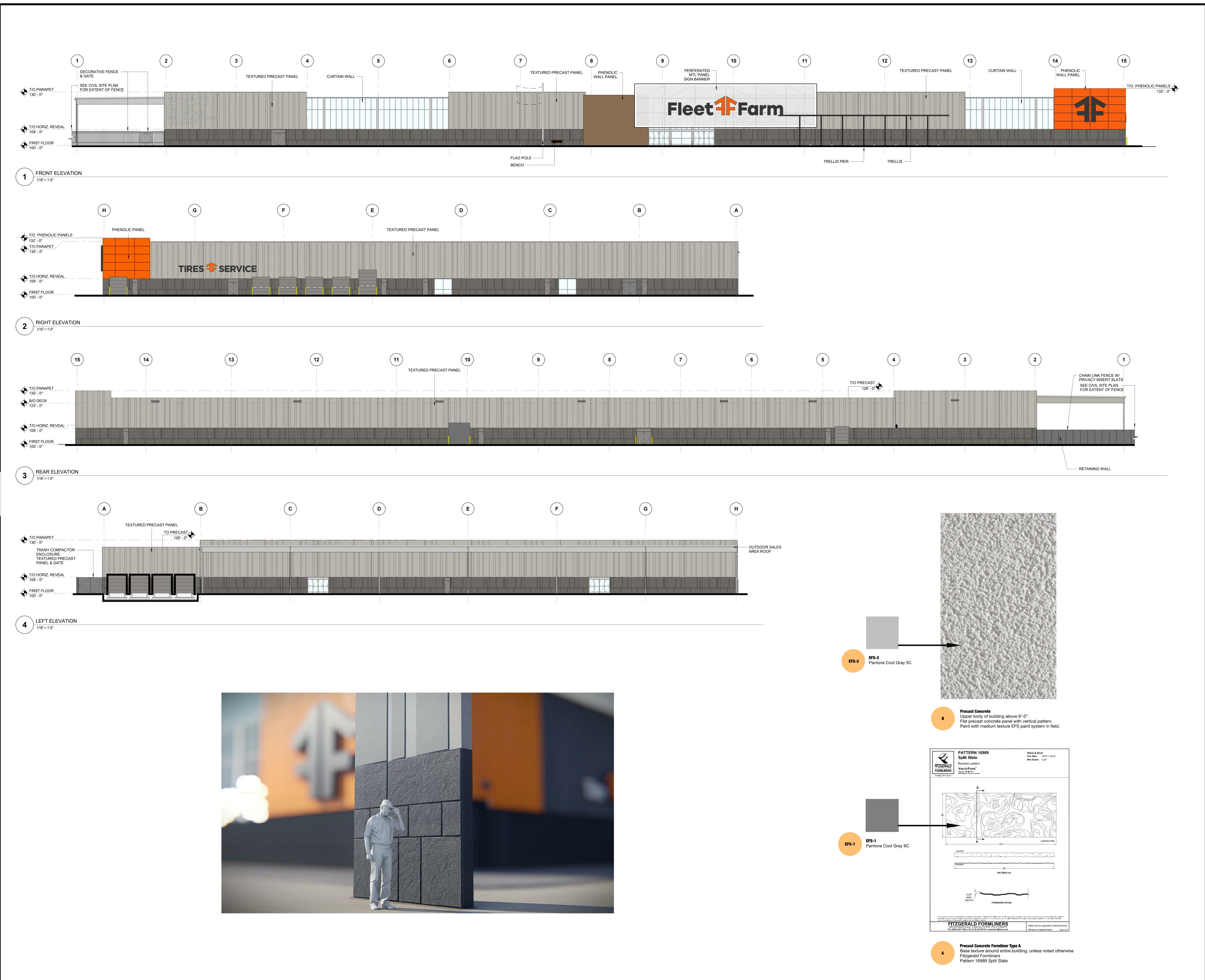


Project	No.	295210700
Drawn E	Зу	BGW
Checked	d By	NHN
Date		07/09/18
all technic proprietary exploited, permissior review an governmer accordanc	cal drawings, y and cannot b in whole or n of RSP Archit d evaluation nt agencies, v e with this No	nown and described herein including graphics, and models thereof, are e copied, duplicated or commercially in part, without express written tects. These are available for limited by clients, consultants, contractors, rendors and office personnel only in tice. tects 2018. All rights reserved.
Sheet Is	sues / Revi	Sions

neet Is	sues / Revisi	ons
No.	Date Description	

CODE REVIEW/ EXIT DIAGRAM

Δ021







rtification

Name	Robert Lucius
License Number	3570
Date Signed	

CEDAR FALLS, IA



Project No.

Drawn By

BGW

Checked By

NHN

Date

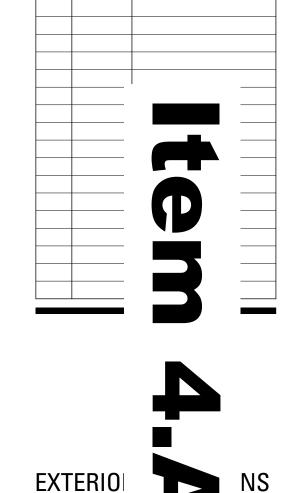
07/09/18

NOTICE: The designs shown and described herein including all technical drawings, graphics, and models thereof, are proprietary and cannot be copied, duplicated or commercially exploited, in whole or in part, without express written permission of RSP Architects. These are available for limited review and evaluation by clients, consultants, contractors, government agencies, vendors and office personnel only in accordance with this Notice.

© Copyright RSP Architects 2018. All rights reserved.

Sheet Issues / Revisions

No. Date Description



C:\Revit Projects\A17_FF_Cedar Falls_04_detached_BWilson.rvt 8/10/2018 5:15:04 PM

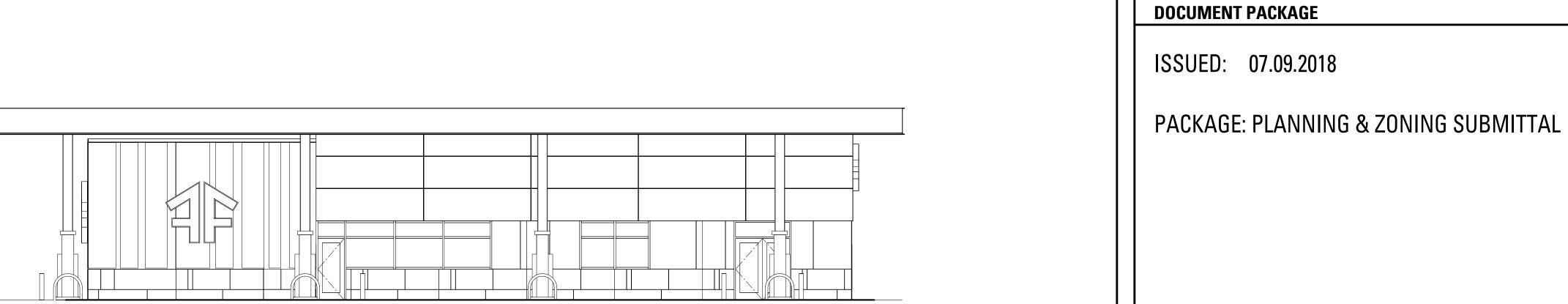
CEDAR FALLS, IA

FLEET FARM - C-STORE

Henry Property

PROJECT NUMBER: 295210700





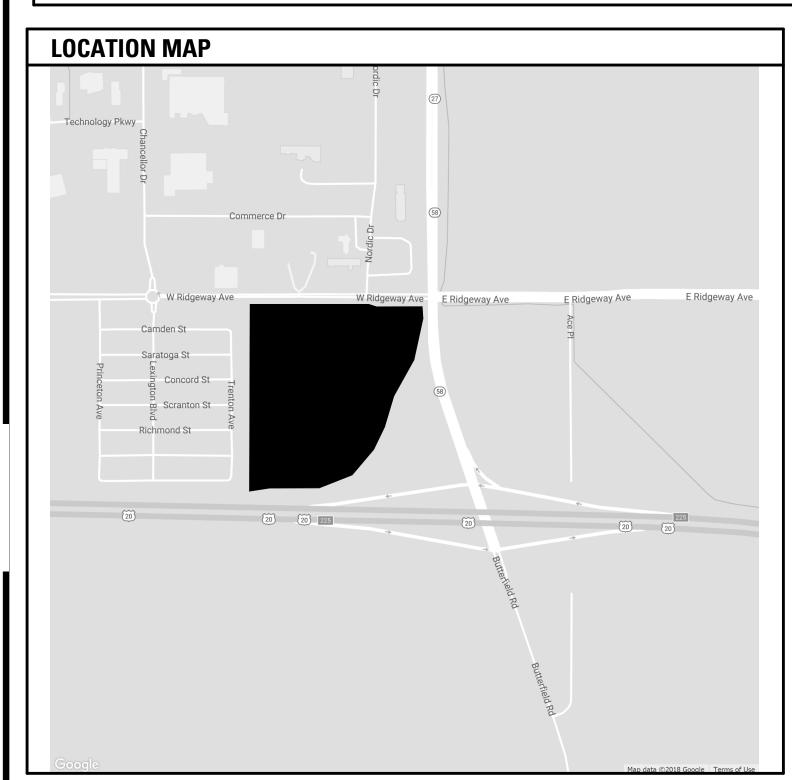


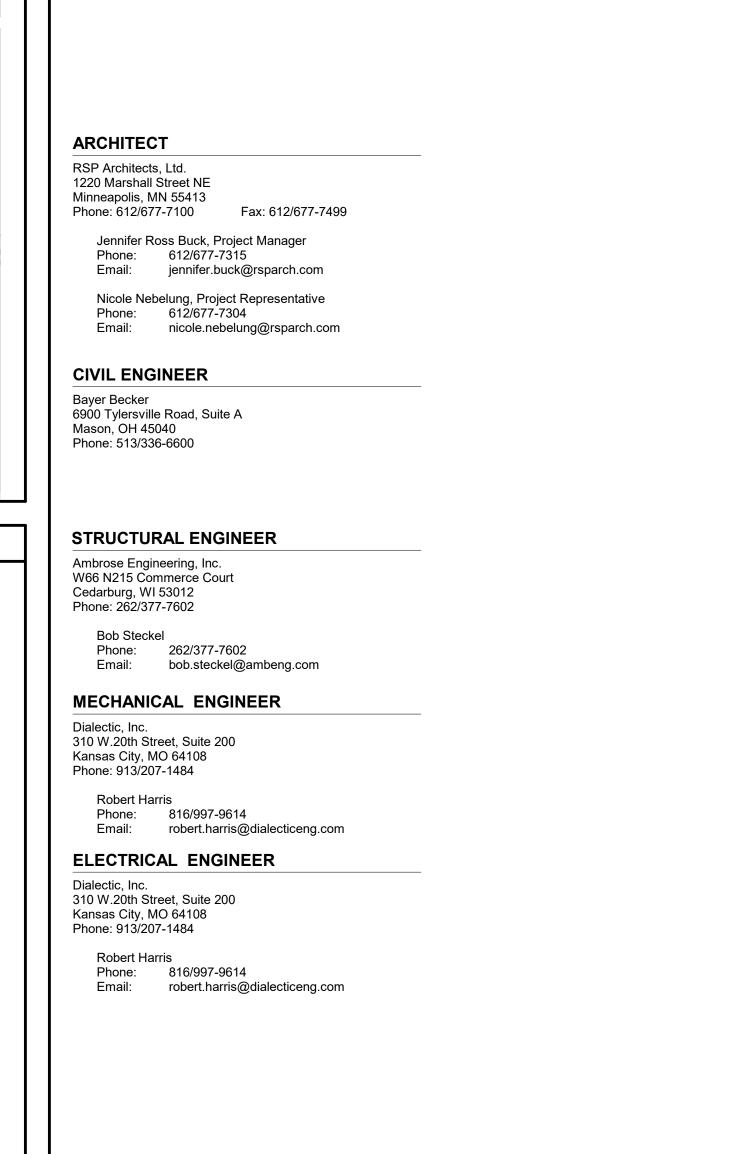


FLEET FARM - C-STORE

CEDAR FALLS, IA







PROJECT TEAM

Cincinnati, OH 45236

Phone: 513/792-5000

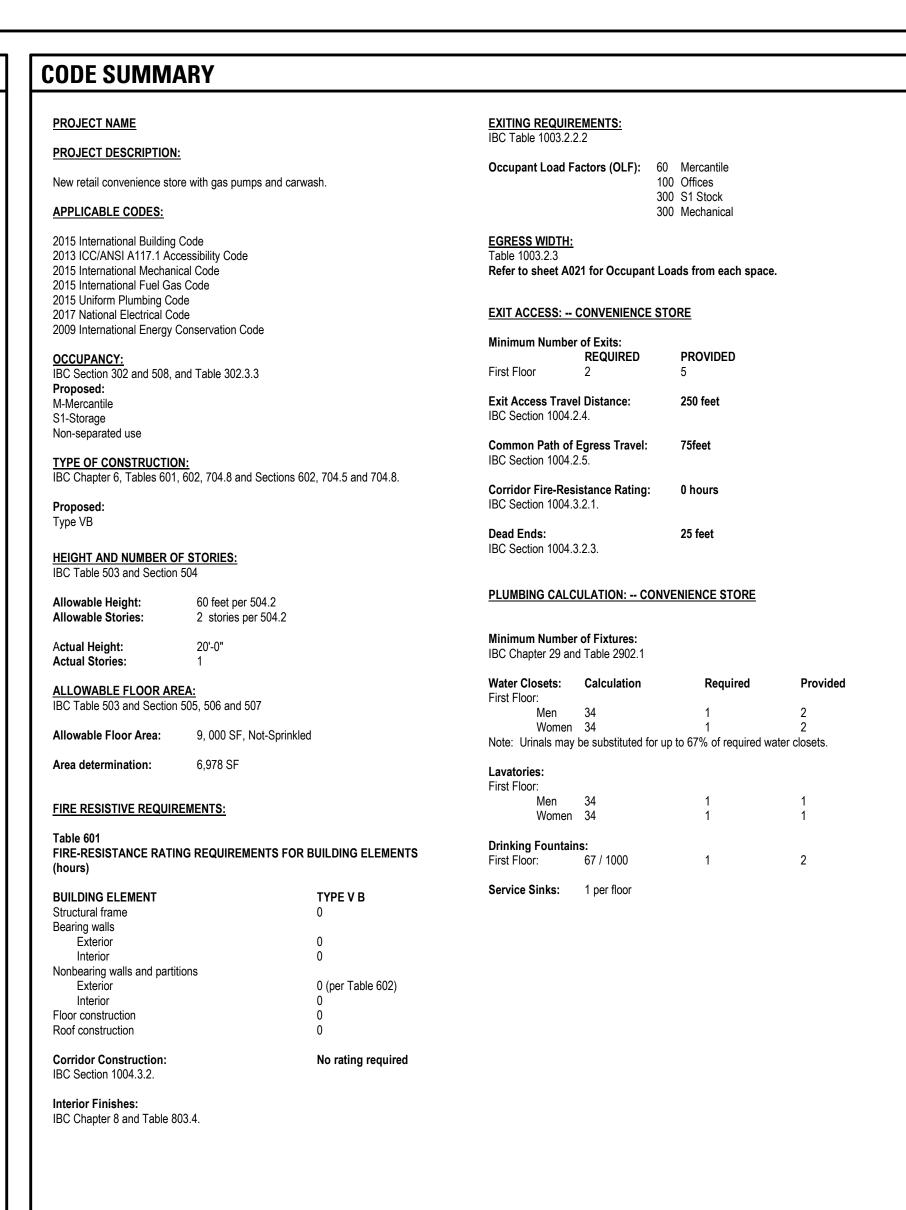
CONTRACTOR

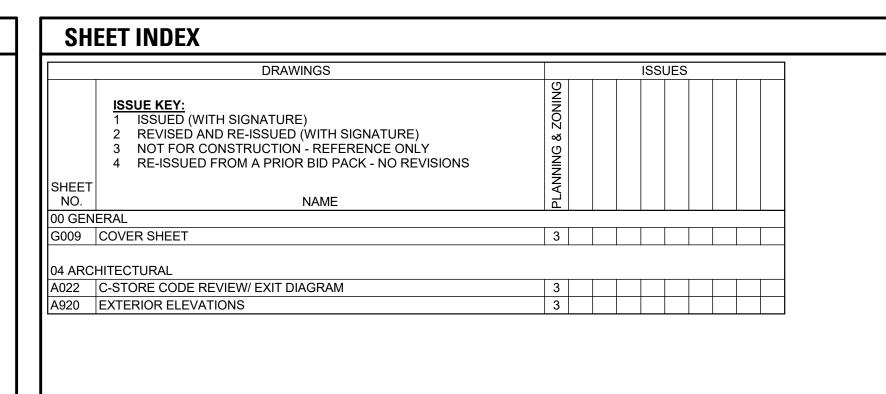
8044 Montgomery Road, Suite 370

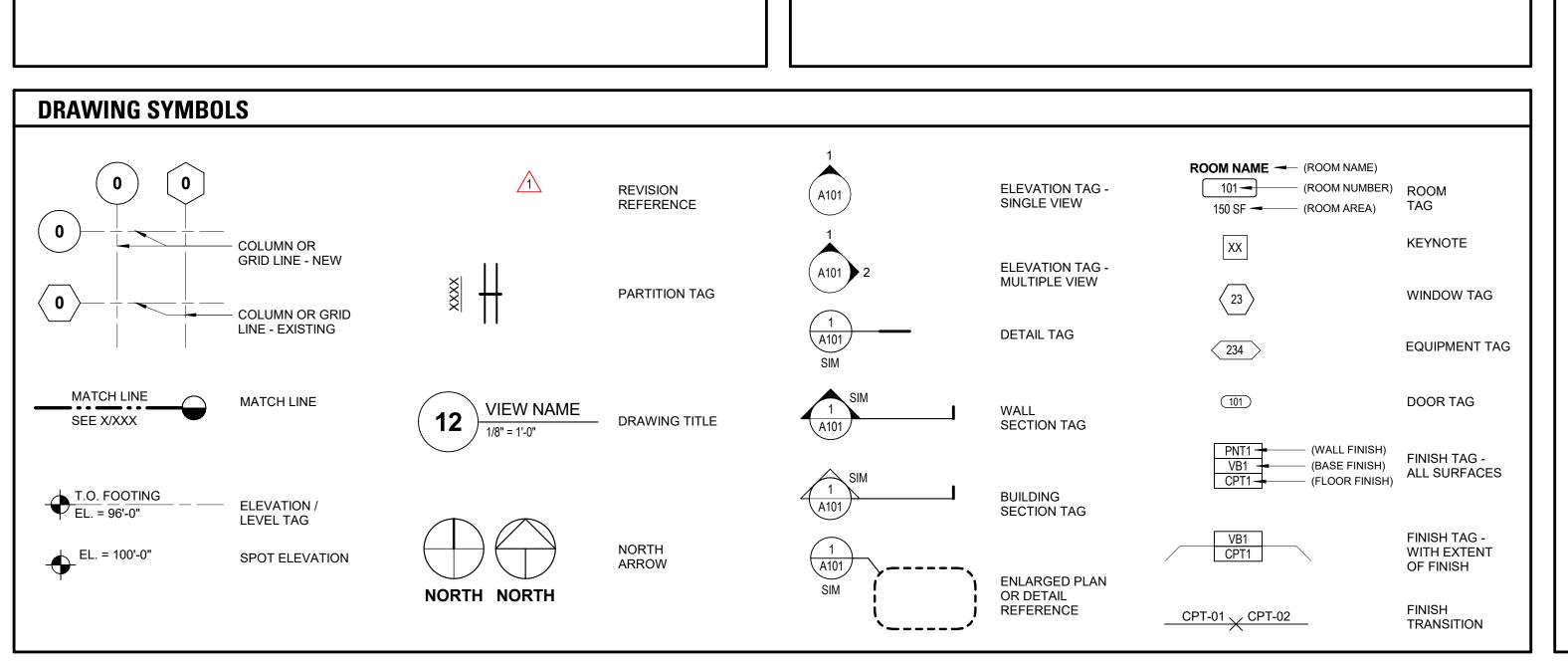
Fleet Farm C/O Midland Atlantic Development Co. LLC

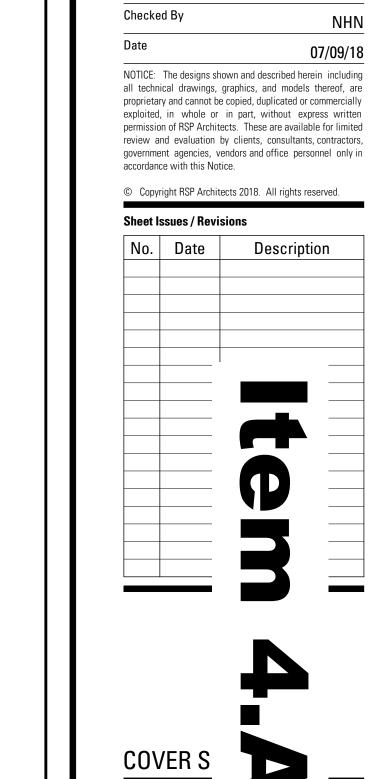
Nicole Chimento Dracon, Director of Leasing

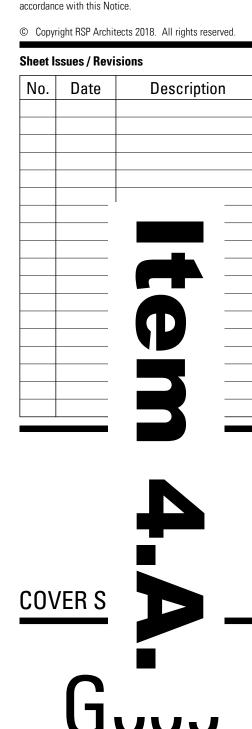
Email: nchimento@midlandatlantic.com





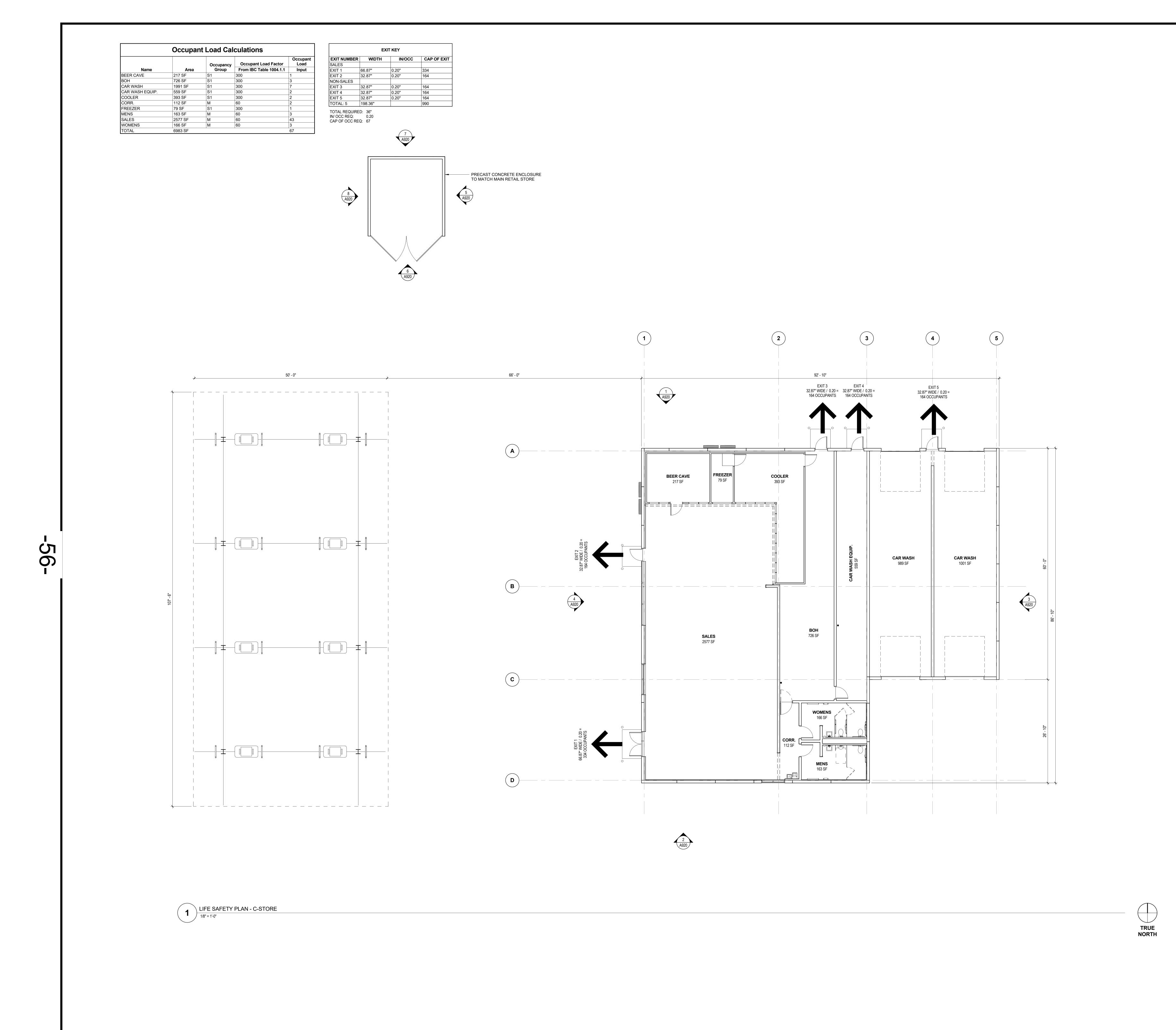






295210700

SITE PLAN



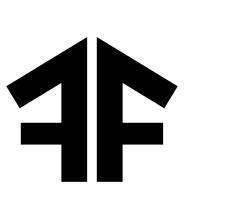




Name	Robert Lucius
License Number	3570
Date Signed	

CEDAR FALLS, IA

FLEET FARM - C-STORE



Project No.	295210700
Drawn By	BGW
Checked By	NHN
Date	07/09/18
all technical drawings, gra proprietary and cannot be co exploited, in whole or in permission of RSP Architects review and evaluation by	n and described herein including phics, and models thereof, are pied, duplicated or commercially part, without express written s. These are available for limited clients, consultants, contractors, lars and office, possennel, only in

© Copyright RSP Architects 2018. All rights reserved.				
Sheet Issues / Revisions				
No.	Date	Description		
	l l			

C-STORE CODE REVIEW/ EXIT DIAGRAM

۸022

C:\Revit Projects\A17_FF_Cedar Falls_C-Store_04_detached_BWilson.rvt

8/10/2018 5:26:55 PM





Name	Robert Lucius
License Number	3570
Date Signed	
Project For	

CEDAR FALLS, IA

HEET EADNA C STODE



Project No.

Drawn By

BGW

Checked By

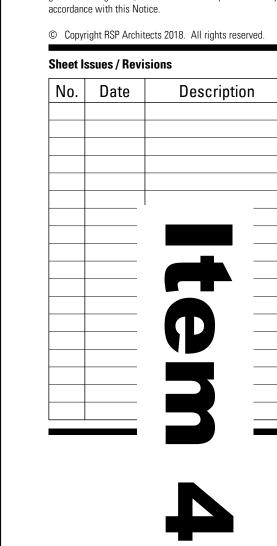
NHN

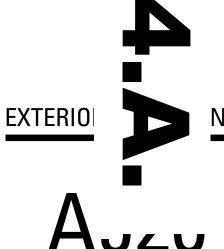
Date

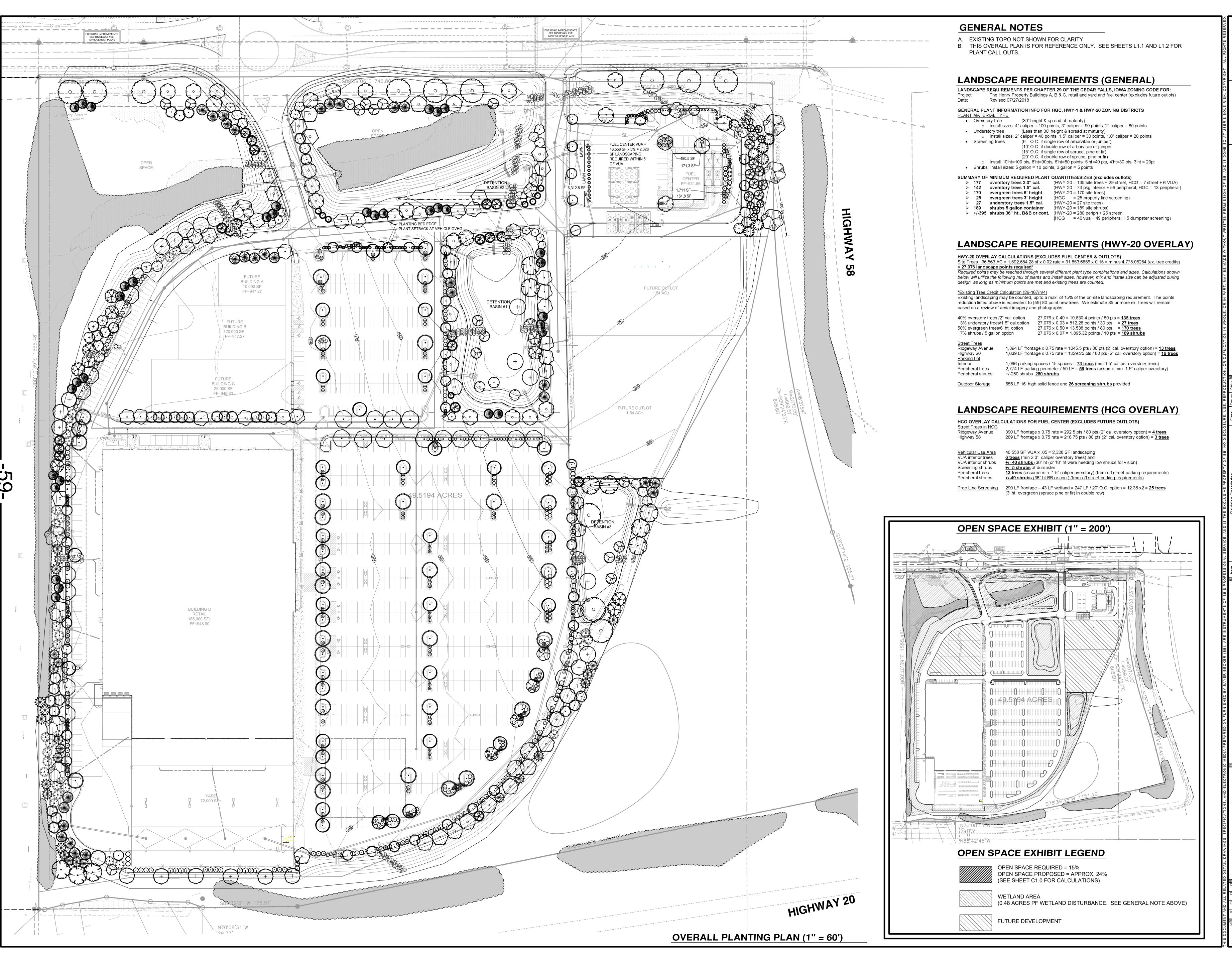
07/09/18

NOTICE: The designs shown and described herein including all technical drawings, graphics, and models thereof, are proprietary and cannot be copied, duplicated or commercially exploited, in whole or in part, without express written permission of RSP Architects. These are available for limited review and evaluation by clients, consultants, contractors, government agencies, vendors and office personnel only in accordance with this Notice.

© Copyright RSP Architects 2018. All rights reserved.







Basis of Bearing:
State Plane NAD27 North

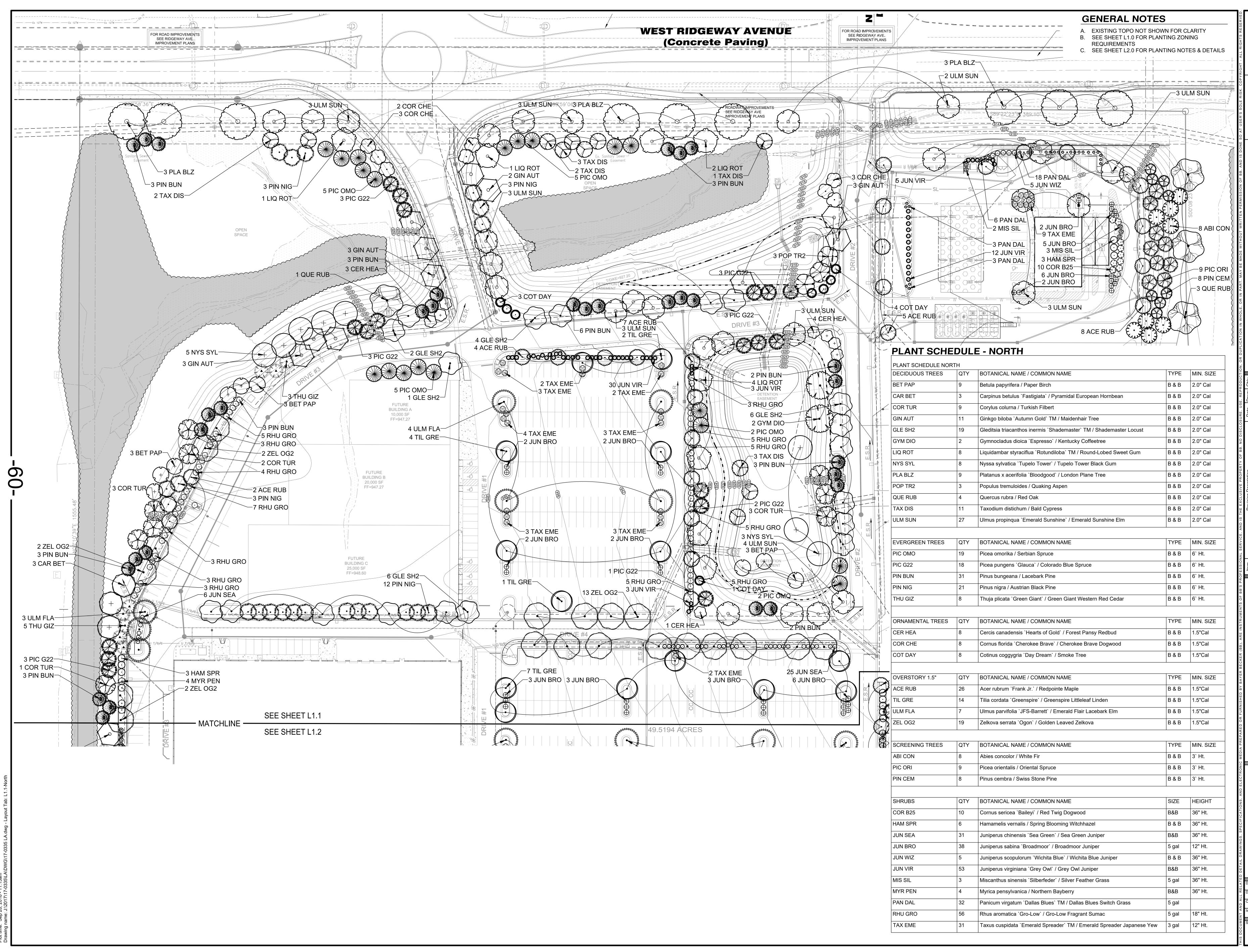
60

SCALE: 1" = 60'

2 REVISED PER STAFF COMMENTS DATED 8/3/2018 08-10-18 HMW
3 REVISED PER CITY, DEVELOPER AND INTERNAL REVIEW COMMENTS 09-28-18 HMW

HENKY PROPEKTY
BLACK HAWK COUNTY
CITY OF CEDAR FALLS
CEDAR FALLS, IOWA

Drawing:
Drawn by:
Checked By:
Issue Date:



Ba State I O SC.

tem Revision Description Date Drwn: Chk:

ReviseD PER CIVIL CHANGES & OUTLOT ACREAGE REMOVED 07-27-18 HMW RLG

REVISED PER CITY, DEVELOPER AND INTERNAL REVIEW COMMENTS 09-28-18 HMW RLG

ReviseD PER CITY, DEVELOPER AND INTERNAL REVIEW COMMENTS 09-28-18 HMW RLG

ReviseD PER CITY, DEVELOPER AND INTERNAL REVIEW COMMENTS 09-28-18 HMW RLG

ReviseD PER CITY, DEVELOPER AND INTERNAL REVIEW COMMENTS 09-28-18 HMW RLG

ReviseD PER CITY, DEVELOPER AND INTERNAL REVIEW COMMENTS 09-28-18 HMW RLG

ReviseD PER CITY, DEVELOPER AND INTERNAL REVIEW COMMENTS 09-28-18 HMW RLG

ReviseD PER CITY, DEVELOPER AND INTERNAL REVIEW COMMENTS 09-28-18 HMW RLG

ReviseD PER CITY, DEVELOPER AND INTERNAL REVIEW COMMENTS 09-28-18 HMW RLG

HENRY PROPERTY
BLACK HAWK COUNTY
CITY OF CEDAR FALLS
CEDAR FALLS, IOWA

becker.com
6900 Tylersville Road, Suite A
Mason, OH 45040 - 513.336.6600

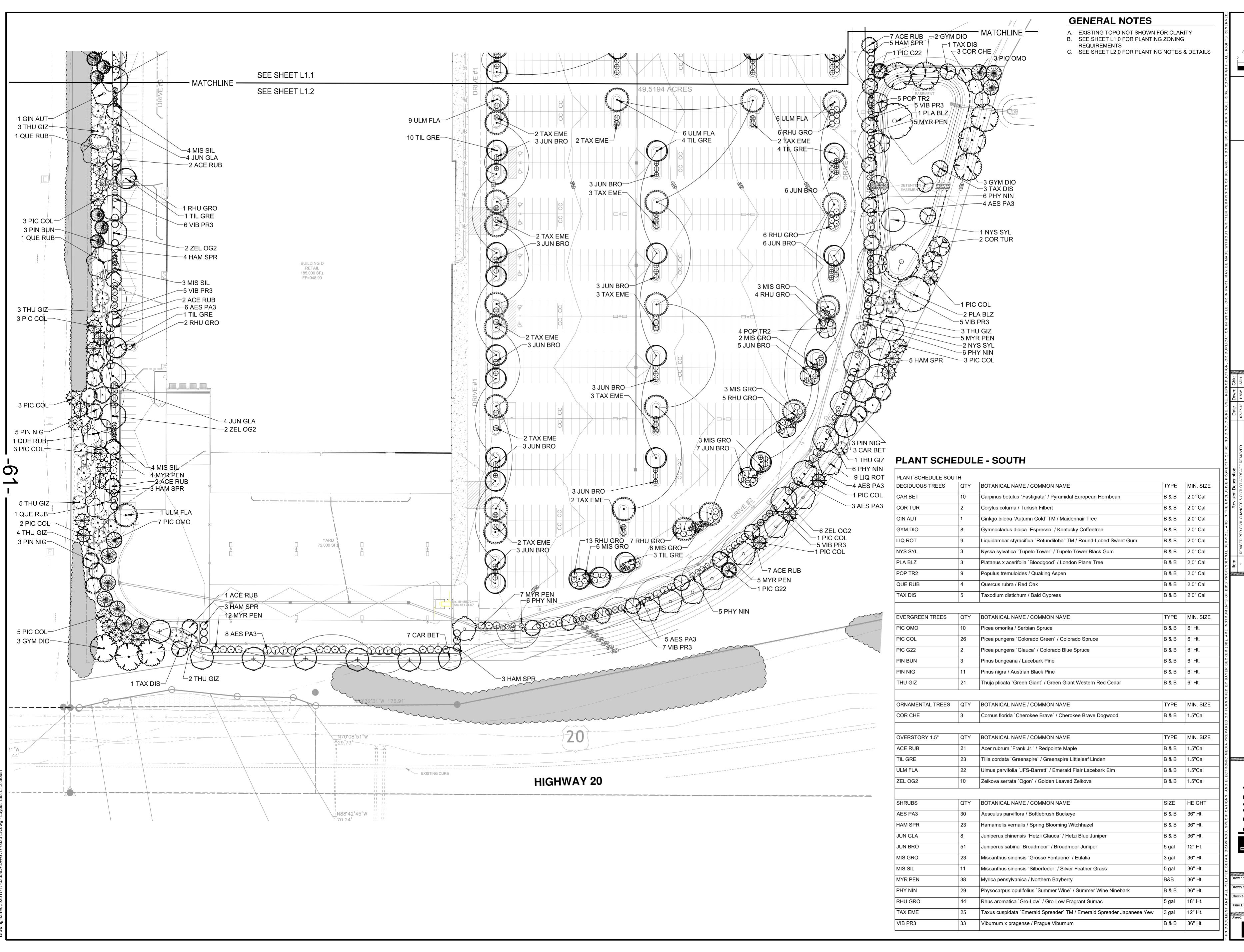
awing:

17-0335 LA
awn by:

HMW
ecked By:

ADH
de Date:

07/09/2018
eet:



Basis of Bearing: State Plane NAD27 North 0 40

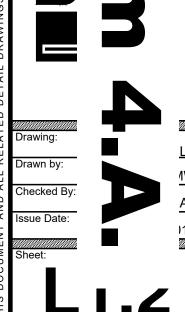
 Revision Description
 Date
 Drwn:
 Chł

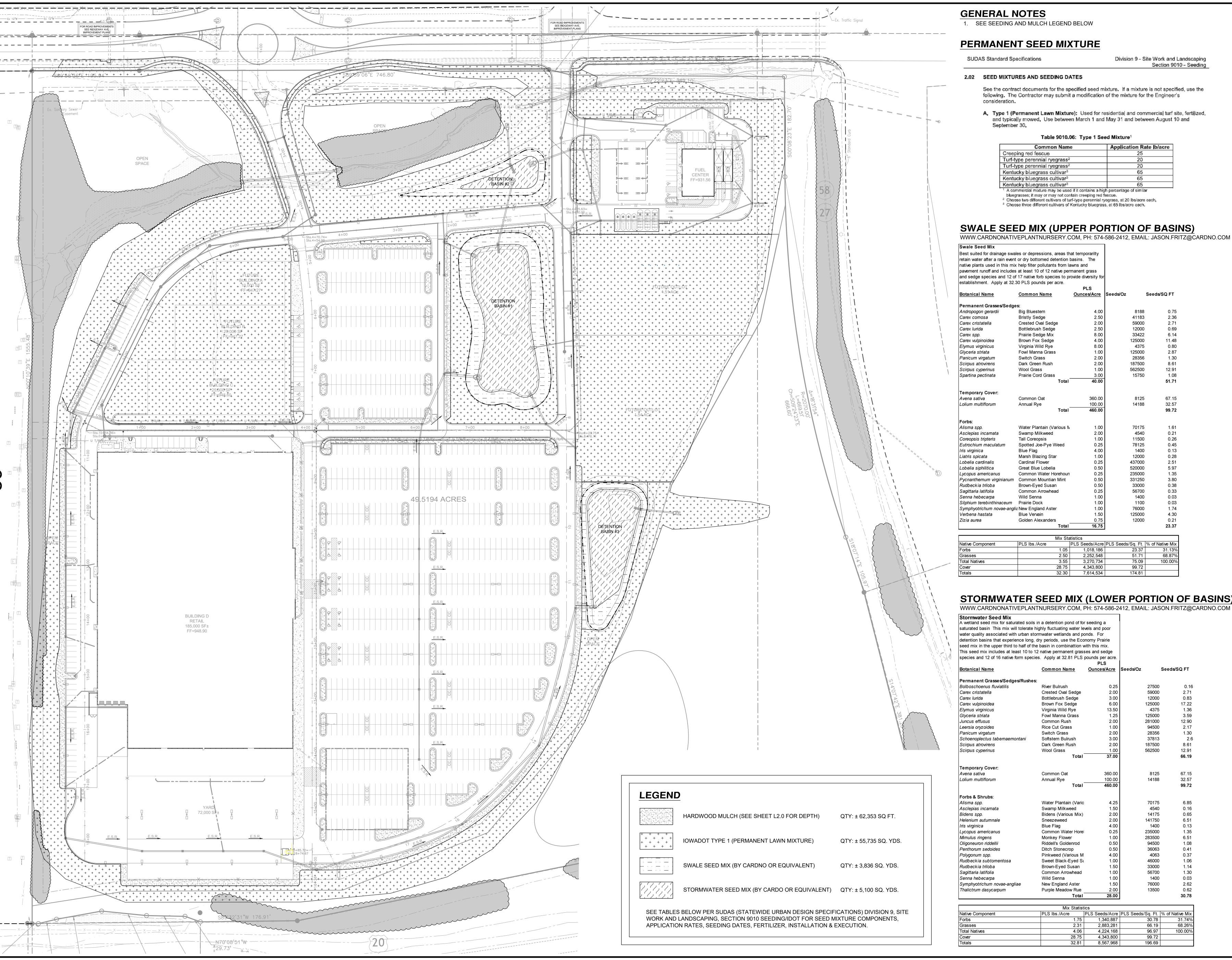
 ED PER CIVIL CHANGES & OUTLOT ACREAGE REMOVED
 07-27-18
 HMW
 ADH

 ED PER STAFF COMMENTS DATED 8/3/2018
 08-10-18
 HMW
 RLC

 ED PER CITY, DEVELOPER AND INTERNAL REVIEW COMMENTS
 09-28-18
 HMW
 RLC

BLACK HAWK COUNTY CITY OF CEDAR FALLS CEDAR FALLS, IOWA





Division 9 - Site Work and Landscaping Section 9010 - Seeding

See the contract documents for the specified seed mixture. If a mixture is not specified, use the following. The Contractor may submit a modification of the mixture for the Engineer's

A. Type 1 (Permanent Lawn Mixture): Used for residential and commercial turf site, fertilized, and typically mowed. Use between March 1 and May 31 and between August 10 and

Common Name	Application Rate lb/acre
Creeping red fescue	25
Turf-type perennial ryegrass ²	20
Turf-type perennial ryegrass ²	20
Kentucky bluegrass cultivar ³	65
Kentucky bluegrass cultivar ³	65
Kentucky bluegrass cultivar ³	65
1. A company solid polyture may be used if it contain	an a biala compations of similar

	Botamcar Name	Common Name	<u>Ounces/Acre</u>	SeeusiOZ	Seeus/SQ F1
	Permanent Grasses/Sedge	·s:			
	Andropogon gerardii	Big Bluestem	4.00	8188	0.75
	Carex comosa	Bristly Sedge	2.50	41183	2.36
	Carex cristatella	Crested Oval Sedge	2.00	59000	2.71
	Carex Iurida	Bottlebrush Sedge	2.50	12000	0.69
	Carex spp.	Prairie Sedge Mix	8.00	33422	6.14
	Carex vulpinoidea	Brown Fox Sedge	4.00	125000	11.48
	Elymus virginicus	Virginia Wild Rye	8.00	4375	0.80
	Glyceria striata	Fowl Manna Grass	1.00	125000	2.87
	Panicum virgatum	Switch Grass	2.00	28356	1.30
	Scirpus atrovirens	Dark Green Rush	2.00	187500	8.61
١	Scirpus cyperinus	Wool Grass	1.00	562500	12.91
١	Spartina pectinata	Prairie Cord Grass	3.00	15750	1.08
		Total	40.00		51.71
	Temporary Cover:				
	Avena sativa	Common Oat	360.00	8125	67.15
	Lolium multiflorum	Annual Rye	100.00	14188	32.57
		Total	460.00		99.72
	Forbs:				
	Alisma spp.	Water Plantain (Various N	1.00	70175	1.61
	Asclepias incamata	Swamp Milkweed	2.00	4540	0.21
	Coreopsis tripteris	Tall Coreopsis	1.00	11500	0.26
	Eutrochium maculatum	Spotted Joe-Pye Weed	0.25	78125	0.45
	Iris virginica	Blue Flag	4.00	1400	0.13
	Liatris spicata	Marsh Blazing Star	1.00	12000	0.28
	Lobelia cardinalis	Cardinal Flower	0.25	437000	2.51
	Lobelia siphilitica	Great Blue Lobelia	0.50	520000	5.97
	Lycopus americanus	Common Water Horehoun	0.25	235000	1.35
	Pycnanthemum virginianum	Common Mountian Mint	0.50	331250	3.80
	Rudbeckia triloba	Brown-Eyed Susan	0.50	33000	0.38
	Sagittaria latifolia	Common Arrowhead	0.25	56700	0.33
	Senna hebecarpa	Wild Senna	1.00	1400	0.03
	Silphium terebinthinaceum	Prairie Dock	1.00	1100	0.03
	Symphyotrichum novae-angli	εNew England Aster	1.00	76000	1.74
	Verbena hastata	Blue Vervain	1.50	125000	4.30

	Mix Statistics					
Native Component	PLS lbs./Acre		PLS Seeds/Acre	PLS Seeds/Sq. Ft.	% of Native Mix	
Forbs		1.05	1,018,186	23.37	31.13%	
Grasses		2.50	2,252,548	51.71	68.87%	
Total Natives		3.55	3,270,734	75.09	100.00%	
Cover		28.75	4,343,800	99.72		
		22.22	7.044.504	474.04		

STORMWATER SEED MIX (LOWER PORTION OF BASINS)

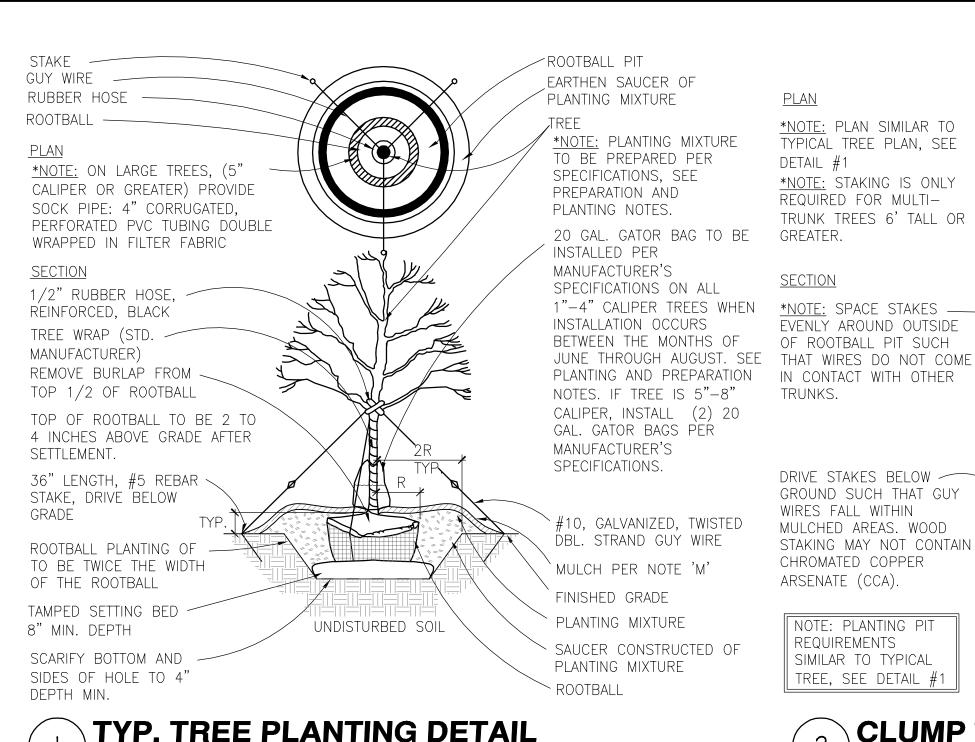
0.21

0.62

30.78

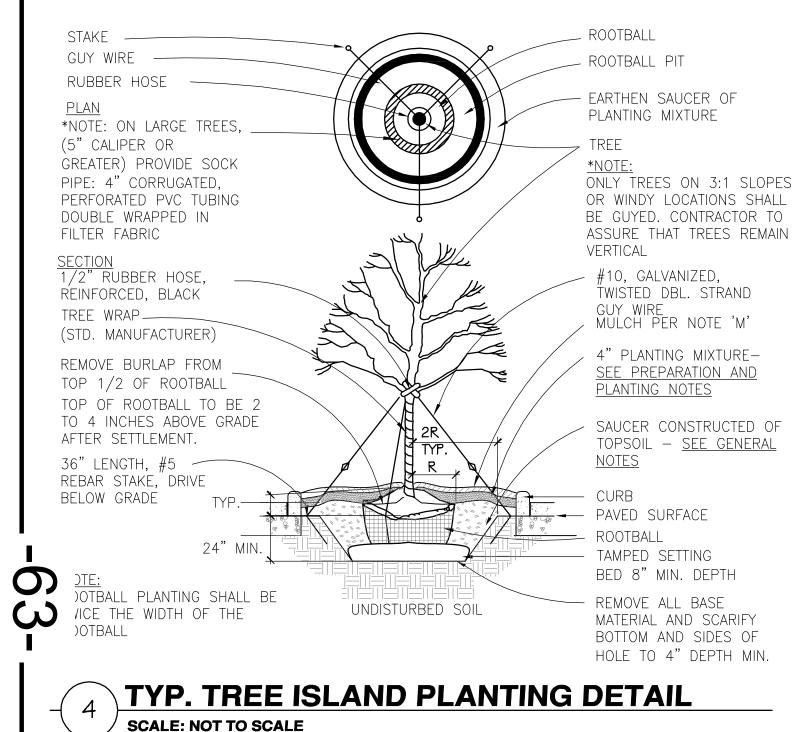
		PLS		
Botanical Name	Common Name	Ounces/Acre	Seeds/Oz	Seeds/SQ FT
Bown and Crease of Sadra of Bushasi				
Permanent Grasses/Sedges/Rushes: Bolboschoenus fluviatilis	River Bulrush	0.25	27500	0.16
Carex cristatella		2.00	59000 59000	2.71
Carex Cristateria Carex lurida	Crested Oval Sedge	3.00	12000	0.83
	Bottlebrush Sedge Brown Fox Sedge	6.00	125000	17.22
Carex vulpinoidea	•	13.50	4375	17.22
Elymus virginicus	Virginia Wild Rye Fowl Manna Grass			3.59
Glyceria striata		1.25	125000	
Juncus effusus	Common Rush	2.00 1.00	281000	12.90
Leersia oryzoides	Rice Cut Grass		94500	2.17
Panicum virgatum	Switch Grass	2.00	28356	
Schoenoplectus tabemaemontani	Softstem Bulrush	3.00	37813	
Scirpus atrovirens	Dark Green Rush	2.00	187500	8.61
Scirpus cyperinus	Wool Grass	1.00	562500	12.91
	Total	37.00		66.19
Temporary Cover:				
Avena sativa	Common Oat	360.00	8125	67.15
Lolium multiflorum	Annual Rye	100.00	14188	32.57
	Total	460.00		99.72
Forbs & Shrubs:				
Alisma spp.	Water Plantain (Vario	4.25	70175	6.85
Asclepias incarnata	Swamp Milkweed	1.50	4540	0.16
Bidens spp.	Bidens (Various Mix)	2.00	14175	0.65
Helenium autumnale	Sneezeweed	2.00	141750	6.51
Iris virginica	Blue Flag	4.00	1400	0.13
Lycopus americanus	Common Water Hore	0.25	235000	1.35
Mimulus ringens	Monkey Flower	1.00	283500	6.51
Oligoneuron riddellii	Riddell's Goldenrod	0.50	94500	1.08
Penthorum sedoides	Ditch Stonecrop	0.50	36063	0.41
Polygonum spp.	Pinkweed (Various M	4.00	4063	0.37
Rudbeckia subtomentosa	Sweet Black-Eyed Su	1.00	46000	1.06
Rudbeckia triloba	Brown-Eyed Susan	1.50	33000	1.14
Sagittaria latifolia	Common Arrowhead	1.00	56700	1.30
Senna hebecarpa	Wild Senna	1.00	1400	0.03
Symphyotrichum novae-angliae	New England Aster	1.50	76000	2 62

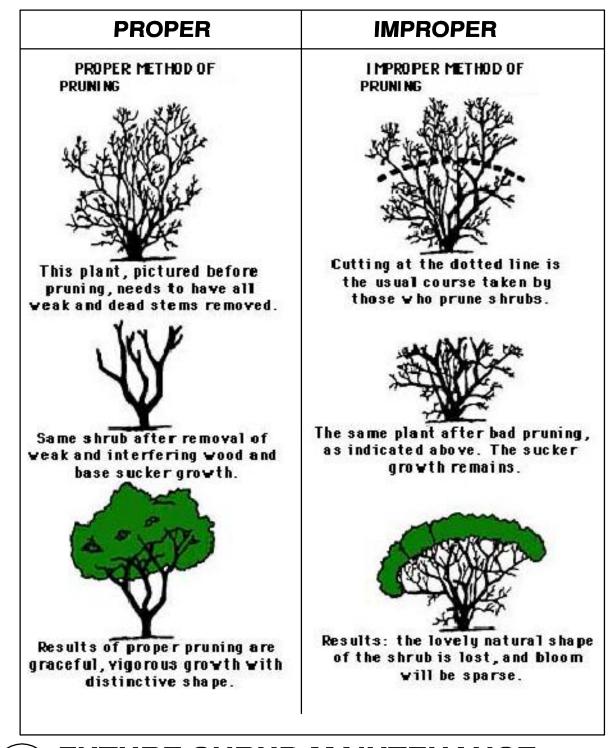
Mix Statistics						
ative Component	PLS lbs./Acre	PLS Seeds/Acre	PLS Seeds/Sq. Ft.	% of Native Mix		
orbs	1.75	1,340,887	30.78	31.74%		
rasses	2.31	2,883,281	66.19	68.26%		
otal Natives	4.06	4,224,168	96.97	100.00%		
over	28.75	4,343,800	99.72			
otals	32.81	8,567,968	196.69			



TYP. TREE PLANTING DETAIL

SCALE: NOT TO SCALE





2" MIN

UNDISTURBED SOIL

<u>*NOTE:</u> PLAN SIMILAR TO

TYPICAL TREE PLAN, SEE

*NOTE: STAKING IS ONLY

TRUNK TREES 6' TALL OR

EVENLY AROUND OUTSIDE

DRIVE STAKES BELOW

WIRES FALL WITHIN

CHROMATED COPPER

REQUIREMENTS

ARSENATE (CCA).

MULCHED AREAS, WOOD

NOTE: PLANTING PIT

SIMILAR TO TYPICAL

TREE, SEE DETAIL #1

SCALE: NOT TO SCALE

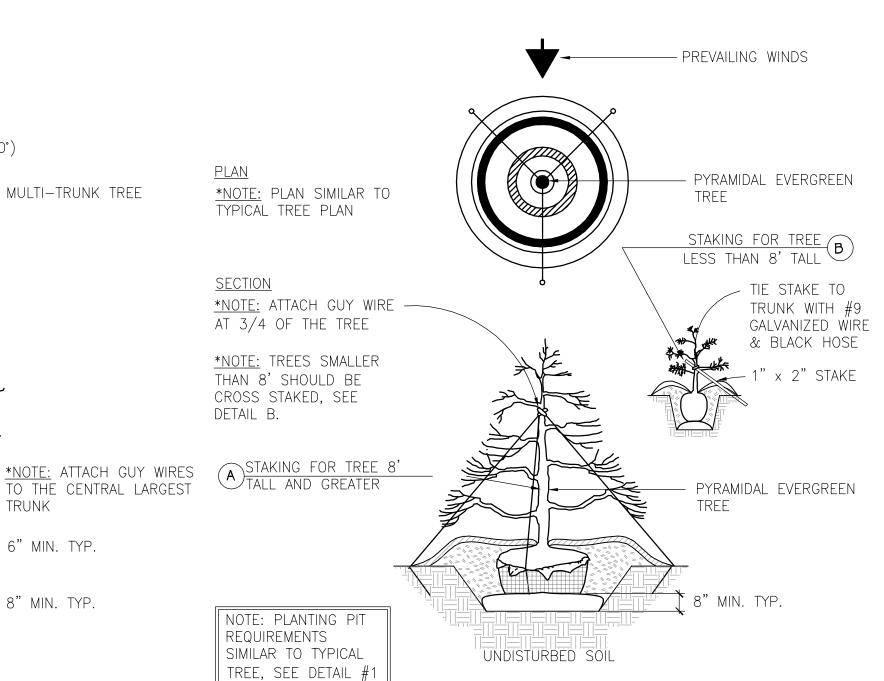
GROUND SUCH THAT GUY

STAKING MAY NOT CONTAIN

REQUIRED FOR MULTI-

DETAIL #1

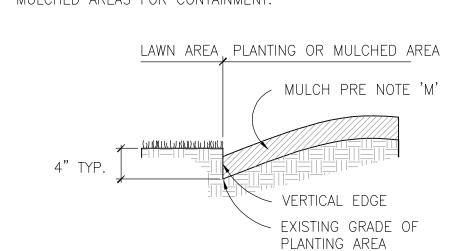
FUTURE SHRUB MAINTENANCE PROPER PRUNING, DO NOT ALTER NATURAL SHAPE OF PLANT



CLUMP TREE PLANTING DETAIL EVERGREEN TREE DETAIL **SCALE: NOT TO SCALE**

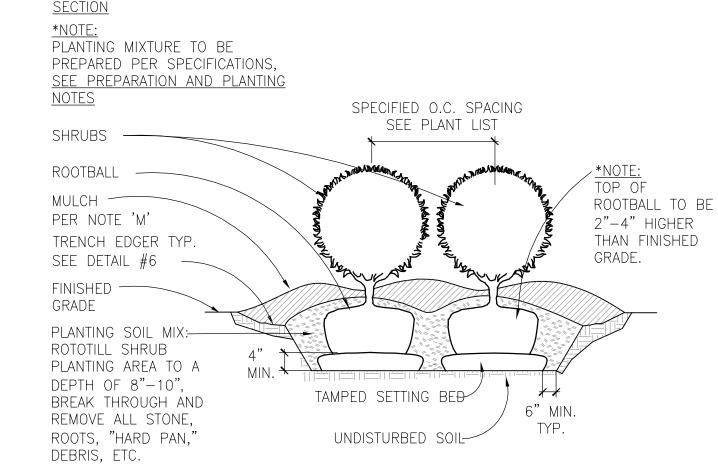
MULTI-TRUNK TREE

. EDGE SHALL CREATE A CLEAN SEPARATION BETWEEN AREAS AND SHALL CREATE SMOOTH AND EVEN LINES AS INDICATED ON PLANS. 2. EDGE DETAIL SHALL BE USED AT ALL LAWN EDGES AND AT EDGES OF MULCHED AREAS FOR CONTAINMENT.



SHRUB BED EDGE DETAIL

SCALE: NOT TO SCALE



SHRUB PLANTING DETAIL

SCALE: NOT TO SCALE

PREPARATION AND PLANTING

- A. NOTIFY LANDSCAPE ARCHITECT OF WORK COMMENCEMENT AND SCHEDULE B. LANDSCAPE CONTRACTOR SHALL LOCATE ALL UNDERGROUND UTILITIES PRIOR TO STARTING WORK. LOCAL UTILITY PROTECTION SERVICES AS NOTED ON THE DRAWINGS. IF A PLANT IS SHOWN ON THE PLAN WITHIN THREE FEET OF AN UNDERGROUND UTILITY, NOTIFY LANDSCAPE ARCHITECT FOR ADJUSTMENT INSTRUCTIONS
- C. PROTECT STRUCTURES, UTILITIES, SIDEWALKS, PAVEMENTS, OTHER FACILITIES, LAWNS, AND EXISTING EXTERIOR PLANTS FROM DAMAGE CAUSED BY PLANTING OPERATIONS. PROVIDE EROSION-CONTROL MEASURES TO PREVENT SOIL EROSION OR DISPLACEMENT AND DISCHARGE OF SOIL—BEARING RUNOFF OR AIRBORNE DUST TO ADJACENT PROPERTIES
- E. LANDSCAPE CONTRACTOR SHALL LAY OUT INDIVIDUAL TREE AND SHRUB LOCATIONS AND BED EDGES FOR ALL EXTERIOR PLANTINGS. STAKE LOCATIONS, OUTLINE AREAS, ADJUST LOCATIONS WHEN REQUESTED, AND OBTAIN LANDSCAPE ARCHITECT'S, OWNER AND/OR THE OWNER'S REPRESENTATIVE WRITTEN ACCEPTANCE OF LAYOUT BEFORE PLANTING. MAKE
- MINOR ADJUSTMENTS AS REQUIRED PER SITE CONDITIONS TREES SHALL BE SITED IN FIELD BY LANDSCAPE ARCHITECT WHERE NOTED ON PLANS. G. ALL PLANTING BEDS ARE TO BE PREPARED AS FOLLOWS - LOOSEN SUBGRADE TO A DEPTH OF 6 - 7". REMOVE STONES LARGER THAN 1" IN ANY DIMENSION AND STICKS, ROOTS, RUBBISH, AND OTHER EXTRANEOUS MATTER,
- AND LEGALLY DISPOSE OF THEM OFF OWNER'S PROPERTY. - SPREAD COMPOST AT A DEPTH OF 4" AND PEAT MOSS AT A DEPTH OF 1". - TILL WITH LOOSENED SUBGRADE MIXING THOROUGHLY.
- GRADE PLANTING BEDS TO A SMOOTH, UNIFORM SURFACE PLANE FOR A UNIFORM FINE TEXTURE. ROLL AND RAKE, REMOVE RIDGES, AND FILL DEPRESSIONS TO MEET - EDGE BEDS 3" TO 4" DEEP (SEE DETAILS ON THIS SHEET)
- H. LANDSCAPE CONTRACTOR SHALL NOTIFY LANDSCAPE ARCHITECT IF SUBSOIL CONDITIONS SHOW EVIDENCE OF UNEXPECTED WATER SEEPAGE OR RETENTION IN TREE OR SHRUB I. REFER TO TYPICAL PLANTING DETAILS ON THIS SHEET FOR PLANT INSTALLATION.
- THE CONTRACTOR'S RESPONSIBILITY TO ASSURE PLANTS REMAIN IN AN UPRIGHT POSITION UNTIL THE END OF THE WARRANTY PERIOD. CONTRACTOR TO REMOVE STAKES & WIRE. K. PRIOR TO MULCHING, APPLY PRE-EMERGENT HERBICIDE (PREEN OR EQUIVALENT) PER MANUFACTURER'S RECOMMENDATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO:

J. IT IS THE CONTRACTOR'S OPTION WHETHER OR NOT TO STAKE A TREE, BUT IT IS ALSO

- KEEP ADJACENT PAVING AND CONSTRUCTION CLEAN, AND MAINTAIN WORK AREA IN AN ORDERLY CONDITION, FOR DURATION OF PROJECT. - PROTECT EXTERIOR PLANTS FROM DAMAGE DUE TO LANDSCAPE OPERATIONS, OPERATIONS BY OTHER CONTRACTORS AND TRADES, AND OTHERS. MAINTAIN PROTECTION DURING INSTALLATION AND MAINTENANCE PERIODS. TREAT, REPAIR, OR REPLACE DAMAGED EXTERIOR PLANTING. - REMOVE SURPLUS SOIL AND WASTE MATERIAL, INCLUDING EXCESS SUBSOIL, UNSUITABLE SOIL, TRASH, AND DEBRIS, AND LEGALLY DISPOSE OF THEM OFF
- M. ALL SHRUB PLANTING BEDS AND INDIVIDUAL TREES AND SHRUBS ARE TO BE MULCHED AT A DEPTH OF 4". ALL ANNUAL, PERENNIAL, AND/OR GROUNDCOVER PLANTING BEDS ARE TO BE MULCHED AT A DEPTH OF 2".
- N. ALL NEW TREE RINGS SHALL BE <u>five feet in diameter</u> and mulched per note 'm'. DO NOT PILE MULCH UP AROUND THE TREE BARK.
- O. ALL DECIDUOUS TREES SHALL BE WRAPPED WITH STANDARD MANUFACTURER'S TREE WRAP TO PREVENT WINTER DAMAGE WHICH SHALL BE REMOVED AFTER THE FIRST WINTER BY THE LANDSCAPE CONTRACTOR.
- P. ALL PLANT MATERIAL SHALL BE PRUNED AND/OR SHAPED IN ACCORDANCE WITH STANDARD HORTICULTURE PRACTICES TO RETAIN THE NATURAL CHARACTER OF THE PLANT. ALL INJURED, DAMAGED, OR CROSSED BRANCHES SHALL BE REMOVED. DO NOT REMOVE THE TREE LEADER.
- Q. LANDSCAPE CONTRACTOR SHALL INSTALL GATOR BAGS, PER MANUFACTURER'S
- SPECIFICATIONS WITH ALL NON-IRRIGATED TREES. R. LANDSCAPE CONTRACTOR SHALL INSTRUCT OWNER OR OWNER'S REPRESENTATIVE ON WATERING NEEDS OF INSTALLED PLANTINGS.

WARRANTY

- A. CONTRACTOR SHALL PROVIDE OWNER WITH A MINIMUM ONE YEAR WRITTEN WARRANTY FOR LABOR AND MATERIALS. B. CONTRACTOR SHALL WARRANT EXTERIOR PLANTS AGAINST DEFECTS, INCLUDING DEATH AND UNSATISFACTORY GROWTH, EXCEPT FOR DEFECTS RESULTING FROM LACK OF ADEQUATE
- CONTRACTOR'S CONTROL. C. WARRANTY SHALL INCLUDE SPECIFIC WARRANTY PERIODS FROM DATE OF ACCEPTANCE FOR

MAINTENANCE, NEGLECT OR ABUSE BY OWNER, OR INCIDENTS THAT ARE BEYOND

- TREES AND SHRUBS, GROUND COVERS, AND OTHER EXTERIOR PLANTS. D. WARRANTY SHALL BE LIMITED TO ONE REPLACEMENT OF EACH EXTERIOR PLANT, EXCEPT FOR LOSSES OR REPLACEMENTS DUE TO FAILURE OF CONTRACTOR TO COMPLY WITH
- REQUIREMENTS. E. WARRANTY FOR IRRIGATION SERVICE, PLUMBING, & DRAINAGE SHALL BE GUARANTEED FOR A PERIOD OF 1 YEAR AND WILL INCLUDE START-UP, WINTERIZATION, AND SECOND SEASON START-UP. WARRANTY SHALL INCLUDE ALL LABOR, MATERIAL, TOOLS, AND EQUIPMENT AS NECESSARY TO PROVIDE A FUNCTIONING SYSTEM, FREE FROM DEFECTS AND ADJUSTED PROPERLY FOR APPROPRIATE WATER DELIVERY TO ALL PLANT MATERIAL.

TRAFFIC & SAFET\

A. REFER TO BID DOCUMENTS AND COMPLY WITH ALL STATE & LOCAL REQUIREMENTS REGARDING APPROVED WORK TIMES, SCHEDULING OF INSTALLATION, AND ALL OTHER

SODDING

REQUIREMENTS.

- A. TURFGRASS SOD SHALL BE OF GOOD QUALITY, FREE OF WEEDS, DISEASE AND INSECTS AND OF GOOD COLOR AND DENSITY
- B. INDIVIDUAL PIECES OF TURFGRASS SOD SHALL BE CUT TO THE SUPPLIER'S STANDARD WIDTH AND LENGTH. MAXIMUM ALLOWABLE DEVIATION FROM STANDARD WIDTHS AND LENGTHS SHALL BE 5 PERCENT. C. STANDARD SIZE SECTIONS OF TURFGRASS SOD SHALL BE STRONG ENOUGH TO SUPPORT
- THEIR OWN WEIGHT AND RETAIN THEIR SIZE AND SHAPE WHEN SUSPENDED VERTICALLY FROM A FIRM GRASP ON THE UPPER 10 PERCENT OF THE SECTION D. LANDSCAPE CONTRACTOR SHALL SOD ALL SPECIFIED AREAS. THE FINAL GRADE AND
- TOPSOIL WITHIN +/- .10 FEET WILL BE IN PLACE FOR SOD CONTRACTOR. E. TILL AREA TO BE SODDED TO A DEPTH OF 4". RAKE TILLED AREA TO REMOVE DEBRIS 1"
- OR LARGER IN SIZE THAT HAS BEEN BROUGHT TO THE SURFACE DURING TILLING. F. AFTER ALL GRADING HAS BEEN COMPLETED, THE SOIL SHALL BE IRRIGATED WITHIN 12-24 HOURS PRIOR TO LAYING THE TURFGRASS SOD. TURFGRASS SOD SHOULD NOT BE LAID ON SOIL THAT IS DRY AND POWDERY.
- G. THE FIRST ROW OF TURFGRASS SOD SHALL BE LAID IN A STRAIGHT LINE, WITH SUBSEQUENT ROWS PLACED PARALLEL TO, AND TIGHTLY AGAINST, EACH OTHER. LATERAL JOINTS SHALL BE STAGGERED TO PROMOTE MORE UNIFORM GROWTH AND STRENGTH. CARE SHALL BE EXERCISED TO INSURE THAT THE TURF IS NOT STRETCHED OR OVERLAPPED, AND ALL JOINTS ARE BUTTED TIGHT IN ORDER TO PREVENT VOIDS, WHICH WOULD CAUSE AIR-DRYING OF THE ROOTS.
- H. ON SLOPES ARE WHERE EROSION MAY BE A PROBLEM, TURFGRASS SOD SHALL BE LAID WITH STAGGERED JOINTS AND SECURED BY PEGGING
- I. THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR WATERING TURFGRASS SOD IMMEDIATELY DURING AND AFTER INSTALLATION TO PREVENT DRYING. IT SHALL THEN BE THOROUGHLY IRRIGATED TO A DEPTH SUFFICIENT THAT THE UNDERSIDE OF THE NEW TURFGRASS SOD PAD AND SOIL IMMEDIATELY BELOW THE TURFGRASS SOD ARE THOROUGHLY WET (USUALLY 1 INCH OF WATER IS NEEDED). THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING ADEQUATE WATER AVAILABLE AT THE SITE PRIOR TO
- J. LANDSCAPE CONTRACTOR IS TO SET GRADE TO PROMOTE POSITIVE DRAINAGE AWAY FROM THE BUILDING AND TO DETENTION BASINS. K. UNLESS OTHERWISE SPECIFIED, THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE
- FOR MAINTAINING THE ACCEPTED SODDED TURFGRASS AREAS UNTIL THE EFFECTIVE DATE FOR TURF MAINTENANCE OPERATIONS BEGINS. THE EFFECTIVE DATE SHALL BE SPECIFIED IN WRITTEN NOTICE FROM THE GENERAL CONTRACTOR.

IRRIGATIONS SYSTEMS

AND DURING INSTALLATION OF THE TURFGRASS SOD.

- A. GENERAL CONTRACTOR SHALL PROVIDED AN IRRIGATION SYSTEM (IF CLIENT WISHES TO IRRIGATE, VERIFY WITH CLIENT). PLAN SHALL PROVIDED FULL SITE COVERAGE ON THE
- B. COMPLETE DESIGN DRAWINGS & EQUIPMENT TO BE SUBMITTED TO OWNER FOR REVIEW.

*IRRIGATION SYSTEM DESIGN / BUILD NOTES: 1. IRRIGATION SYSTEM SHALL HAVE A REMOVABLE EXTERIOR BACK-FLOW PREVENTER WITH FAKE ROCK COVER IN GRASS AREA OUTSIDE OF METER PIT AT POINT OF CONNECTION.

- 2. IRRIGATION HEADS(DRIPLINE IN PLANTING BEDS; COMPLETE ROTOR SPRINKLER IN LAWN). 3. RAIN BIRD ESP OR HUNTER I-CORE CONTROLLER LOCATED IN MECH ROOM WITH RAIN SENSOR.
- WITH GENERAL CONTRACTOR. 5. IRRIGATION CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR LAYOUT APPROVAL.
- 4. SLEEVES ARE REQUIRED UNDER ALL PAVEMENT / SIDEWALK FOR SYSTEM, COORDINATE

GENERAL LANDSCAPE NOTES

- FULLY UNDERSTAND THE NATURE AND SCOPE OF WORK NEEDED TO ACHIEVE THE FINISHED PRODUCT INTENDED BY THE OWNER. IN ADDITION, THE CONTRACTOR SHALL AT ONCE REPORT TO THE LANDSCAPE ARCHITECT, INACCURACIES OR INCONSISTENCIES DISCOVERED. FAILURE TO REASONABLY RECOGNIZE OR NOTIFY THE LANDSCAPE ARCHITECT OF SUCH ITEMS SHALL RELEASE THE LANDSCAPE ARCHITECT AND OWNER OF ALL LIABILITY. ANY DEVIATIONS FROM
- THESE DOCUMENTS WITHOUT WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT SHALL BE CORRECTED AT THE CONTRACTORS EXPENSE B. PROTECT ALL EXISTING VEGETATION TO REMAIN AS PER PLANS AND SPECIFICATIONS
- . PRIOR TO CONSTRUCTION, THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS NECESSARY TO COMPLETE THE WORK, LOCATING ALL UNDERGROUND UTILITIES, AND SHALL AVOID DAMAGE TO ALL UTILITIES DURING INSTALLATION. THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ALL DAMAGE TO UTILITIES, STRUCTURES, SITE APPURTENANCES, ETC., WHICH MAY OCCUR AS A RESULT OF LANDSCAPE CONSTRUCTION.
 - D. LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS NECESSARY TO COMPLETE THE WORK. E. LANDSCAPE CONTRACTOR MUST CERTIFY THAT ITS SAFETY PROGRAM MEETS REGULATORY REQUIREMENTS AT A MINIMUM. CONTRACTOR TO PROVIDE DOCUMENTATION OF THE OSHA
- RECORD KEEPING SUMMARY. F. LANDSCAPE ARCHITECT MAY OBSERVE TREES AND SHRUBS EITHER AT PLACE OF GROWTH OR AT SITE BEFORE PLANTING FOR COMPLIANCE WITH REQUIREMENTS FOR GENUS, SPECIES VARIETY, SIZE, AND QUALITY. LANDSCAPE ARCHITECT RETAINS RIGHT TO OBSERVE TREES AND SHRUBS FURTHER FOR SIZE AND CONDITION OF BALLS, ROOT SYSTEMS, INSECTS, INJURIES, AND LATENT DEFECTS AND TO REJECT UNSATISFACTORY OR DEFECTIVE MATERIAL AT ANY TIME
- SHRUBS IMMEDIATELY FROM PROJECT SITE G. IF EXISTING ECOLOGY CAUSES ADJUSTMENTS OF LANDSCAPE PLANS TO FIT THE SITE CONDITIONS, A STAKE OUT BY LANDSCAPE CONTRACTOR AND ADJUSTMENTS BY LANDSCAPE

DURING PROGRESS OF WORK. LANDSCAPE CONTRACTOR SHALL REMOVE REJECTED TREES OR

- ARCHITECT SHALL BE REQUIRED PRIOR TO INSTALLATION. H. THE OWNER AND/OR THE OWNER'S REPRESENTATIVE WILL APPROVE STAKED LOCATIONS OF ALL MATERIAL PRIOR TO INSTALLATION, OR AS DEEMED NECESSARY BY OWNER/OWNER'S REP. I. ALL PLANTING AREAS SHOWN ON PLANS SHALL BE TO WITHIN 2" OF FINAL GRADE BEFORE
- LANDSCAPE CONTRACTOR COMMENCES INSTALLATION J. FOR LOCATION AND DESCRIPTION OF PROPOSED SITE UTILITIES, STORM STRUCTURES, EASEMENTS, ETC., REFER TO THE SITE PLAN AND CIVIL ENGINEERING DRAWINGS. K. ALL PLANT MATERIAL MUST BE INSTALLED ACCORDING TO THE APPROVED LANDSCAPING PLAN BY NO LATER THAN THE NEXT PLANTING SEASON OR WITHIN 6 MONTHS FROM THE
- COMPLETION OF ALL SITE CONSTRUCTION. L. PLANT QUANTITIES ARE ESTIMATES ONLY. CONTRACTOR TO VERIFY ALL PLANT QUANTITIES. ANY DISCREPANCY BETWEEN THE PLANTING LIST AND THE PLAN SHALL BE VERIFIED BY THE LANDSCAPE ARCHITECT. ALL SUBSTITUTIONS AND/OR CHANGES SHALL BE REQUESTED IN
- WRITING TO THE OWNER OR OWNER'S REPRESENTATIVE AND BE APPROVED BY THE LANDSCAPE. ARCHITECT AND THE LOCAL MUNICIPALITY PRIOR TO INSTALLATION M. THE CONTRACTOR SHALL PROVIDE QUALITY, SIZE, GENUS, SPECIES, AND VARIETY OF EXTERIOR
- PLANTS INDICATED, COMPLYING WITH APPLICABLE REQUIREMENTS IN ANSI Z60.1, "AMERICAN STANDARD FOR NURSERY STOCK." N. THE CONTRACTOR SHALL FURNISH NURSERY-GROWN TREES AND SHRUBS COMPLYING WITH ANSI Z60.1, WITH HEALTHY ROOT SYSTEMS DEVELOPED BY TRANSPLANTING OR ROOT PRUNING. PROVIDE WELL-SHAPED, FULLY BRANCHED, HEALTHY, VIGOROUS STOCK, FREE OF DISEASE,
- INSECTS, EGGS, LARVAE, AND DEFECTS SUCH AS KNOTS, SUN SCALD, INJURIES, ABRASIONS, AND DISFIGUREMENT. O. TREES AND SHRUBS OF A LARGER SIZE MAY BE USED, IF ACCEPTABLE TO LANDSCAPE
- P. IF FORMAL ARRANGEMENTS OR CONSECUTIVE ORDER OF TREES OR SHRUBS IS SHOWN, SELECT STOCK FOR UNIFORM HEIGHT AND SPREAD, AND NUMBER LABEL TO ASSURE SYMMETRY IN PLANTING.

ARCHITECT, WITH A PROPORTIONATE INCREASE IN SIZE OF ROOTS OR BALLS.

- Q. THE CONTRACTOR SHALL LABEL ONE TREE AND ONE SHRUB OF EACH VARIETY AND CALIPER WITH A SECURELY ATTACHED, WATERPROOF TAG BEARING LEGIBLE DESIGNATION OF BOTH THE BOTANICAL AND COMMON NAME.
- R. TOPSOIL SHALL BE ASTM D 5268. pH RANGE OF 5.5 TO 7. A MINIMUM OF 4 PERCENT ORGANIC MATERIAL CONTENT; FREE OF STONES 1 INCH OR LARGER IN ANY DIMENSION AND OTHER EXTRANEOUS MATERIALS HARMFUL TO PLANT GROWTH. STANDARD TOPSOIL PLANTING MATERIAL SHALL CONSIST OF 60% NATIVE SOIL, 10% PEAT HUMUS, AND 30% COMPOST. S. COMPOST SHALL BE WELL-COMPOSTED, STABILE, AND WEED-FREE ORGANIC MATTER, pH RANGE OF 5.5 TO 8; MOISTURE CONTENT 35 TO 55 PERCENT BY WEIGHT; 100 PERCENT PASSING THROUGH ONE INCH SIEVE; SOLUBLE SALT CONTENT OF 5 TO 10 DECISIEMENS/M;
- PLANTINGS. T. FERTILIZER SHALL BE SLOW-RELEASE, GRANULAR OR PELLETS CONSISTING OF 50 PERCENT WATER-INSOLUBLE NITROGEN. PHOSPHORUS. AND POTASSIUM IN AMOUNTS RECOMMENDED FOR

NOT EXCEEDING 0.5 PERCENT INERT CONTAMINANTS AND FREE OF SUBSTANCES TOXIC TO

- TYPE OF PLANT BEING GROWN. U. MULCH TO BE FREE FROM DELETERIOUS MATERIALS AND DISEASE AND SUITABLE AS A TOP DRESSING OF TREES AND SHRUBS. MULCH SHALL BE CLEAN. NON-DYED. TOXIC FREE.
- CRUSHED ROCK AND PEA GRAVEL OR SIMILAR MATERIALS ARE NOT ACCEPTABLE MULCH/GROUNDCOVER. ALL LANDSCAPE ISLANDS SHALL BE MULCHED BY THE LANDSCAPE CONTRACTOR UNLESS OTHERWISE STATED IN THE LANDSCAPE PLANS.

DOUBLE-SHREDDED HARDWOOD, DARK BROWN IN COLOR, UNLESS NOTED OTHERWISE.

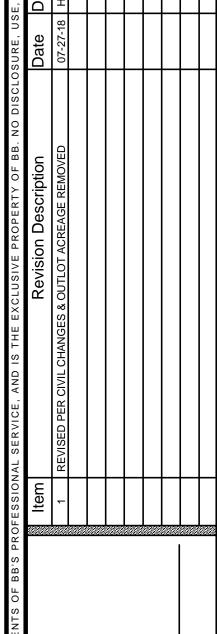
- V. PLANT TAGS SHALL REMAIN ON INSTALLED PLANT MATERIAL UNTIL THE WORK HAS BEEN APPROVED BY LOCAL INSPECTOR AND/OR THE OWNER OR OWNERS REPRESENTATIVE. W. LANDSCAPE CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE FROM ALL PLANT BEDS WITHOUT ADVERSELY AFFECTING SITE DRAINAGE. GRADES BEHIND CURBS FOR AREAS RECEIVING MULCH SHALL BE 3 INCHES BELOW TOP OF CURB & 2 INCHES BELOW TOP OF CURB FOR SOD.
- X. THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR FULLY MAINTAINING, INCLUDING, BUT NOT LIMITED TO, WATERING, SPRAYING, MULCHING, FERTILIZING, ETC., ALL PLANTING AREAS AND LAWN UNTIL THE WORK IS ACCEPTED BY THE OWNER OR THE OWNER'S REPRESENTATIVE. Y. ISLAND AND PLANTER GRADES (IF APPLICABLE) SHALL BE MOUNDED IN THE CENTER SLOPING DOWNWARD FROM THE MIDDLE OUTWARD AT A MINIMUM 2% AND A MAXIMUM 4%.
- Z. ALL PLANT MATERIAL INSTALLATION TO BE COORDINATED WITH IRRIGATION CONTRACTOR. ACTIVATE IRRIGATION SYSTEM UPON COMPLETION OF ALL LAWN AREAS. ALL IRRIGATION COMPONENTS SHALL BE OF THE SAME MANUFACTURER & INSTALLED PER MANUFACTURER
- AA.NO TREES SHALL BE PLANTED WITHIN 10'-0" OF FIREHYDRANTS.

SEEDING

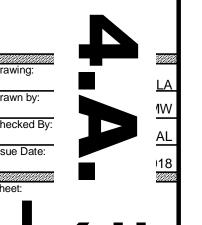
- A. LANDSCAPE CONTRACTOR SHALL SEED ALL DISTURBED AREAS. THE FINAL GRADE AND TOPSOIL WITHIN +/- .10 FEET WILL BE IN PLACE FOR SEEDING CONTRACTOR.
- B. CONTRACTOR SHALL APPLY CELLULOSE FIBER MULCH AT A MINIMUM RATE OF 1500 LBS./ACRE AND FERTILIZERS: BEST 6-20-20 OR BEST 15-15-15 OR APPROVED EQUAL APPLIED AT RATE APPROPRIATE FOR PRODUCT. ORGANIC TACKIFIER SHALL BE APPLIED AT RATE OF 70 LBS./ACRE. HYDROSEED SEED MIX SHALL BE APPLIED AT THE 2,000 LBS./ACRE.
- C. CONTRACTOR SHALL WATER ALL PLANT AREAS THOROUGHLY TO SATURATE UPPER LAYERS OF SOIL PRIOR TO THE HYDROSEEDING OPERATION. ALLOW THE PLANTING AREA SOIL SURFACE TO DRY OUT FOR ONE DAY ONLY PRIOR TO THE HYDROSEEDING APPLICATION. D. CONTRACTOR SHALL APPLY THE HYDROSEEDING IN THE FORM OF A SLURRY CONSISTING OF ORGANIC SOIL AMENDMENTS, COMMERCIAL FERTILIZER, AND ANY OTHER CHEMICALS THAT
 - ARE CALLED OUT. WHEN HYDRAULICALLY SPRAYED ONTO THE SOIL, THE MULCH SHALL FORM A BLOTTER-LIKE MATERIAL. SPRAY THE AREA WITH A UNIFORM VISIBLE COAT, USING THE DARK COLOR OF THE CELLULOSE FIBER AS A VISUAL GUIDE. THE SLURRY SHALL BE APPLIED IN A DOWNWARD DRILLING MOTION VIA A FAN STREAM NOZZLE. CONTRACTOR SHALL INSURE THAT ALL OF THE SLURRY COMPONENTS ENTER AND MIX WITH THE SOIL.
- E. IF SLURRY COMPONENTS ARE LEFT FOR MORE THAN TWO HOURS IN THE MACHINE, ADD 50% MORE OF THE ORIGINALLY SPECIFIED SEED MIX TO ANY SLURRY MIXTURE WHICH HAS NOT BEEN APPLIED WITHIN THE TWO HOURS AFTER MIXING. ADD 75% MORE OF THE ORIGINAL SEED MIX TO ANY SLURRY MIXTURE WHICH HAS NOT BEEN APPLIED EIGHT (8) HOURS AFTER MIXING. ALL MIXTURES MORE THAN EIGHT (8) HOURS OLD, SHALL BE DISPOSED, OFFSITE, AT THE CONTRACTOR'S EXPENSE.
- F. CONTRACTOR SHALL REMOVE ALL SLURRY SPRAYED ONTO HARDSCAPE AREAS INCLUDING CONCRETE WALKS, FENCES, WALLS, BUILDINGS, ETC. AT THE CONTRACTOR'S EXPENSE. G. CONTRACTOR SHALL SAVE ALL SEED AND FERTILIZER TAGS AND FIBER MULCH BAGS FOR THE LANDSCAPE ARCHITECT TO VERIFY COMPLIANCE WITH THE DRAWINGS AND SPECIFICATIONS.

A. ALL WEEDS AND EXISTING VEGETATION MUST BE ELIMINATED PRIOR TO HYDROSEEDING OR DRILL SEEDING ANY NATIVE SEED MIX.

- B. INSTALLATION SHALL BE PERFORMED IN LATE FALL EARLY WINTER OR SPRING. IF SITE IS PREPARED AT ANY OTHER TIME OF THE YEAR, STABILIZE WITH THE FOLLOWING SEED MIX PER ACRE: 64 LBS SEED OATS(AVENA SATIVA)/25 LBS ANNUAL RYE GRASS (LOLIUM MULTIFLORUM) C. IF VEGETATION EXISTS ON SITE, APPLY A GLYPHOSATE HERBICIDE AT LEAST THREE DAYS PRIOR
- TO INSTALLATION ON ALL ACTIVELY GROWING VEGETATION. NEVER APPLY FERTILIZER TO THE D. IF DRILL SEEDING, INSTALL SEED WHEN SOIL IS SUFFICIENTLY DRY SO THAT SOIL DOES NOT STICK TO THE PACKER WHEELS ON THE DRILL.
- E. ENSURE THE DRILL OR HYDROSEEDER IS PROPERLY CALIBRATED TO SOW THE SPECIFIED AMOUNT OF SEED OVER THE SPECIFIED AREA. ENSURE COMPLETE COVERAGE OF THE SPECIFIED AREA.
- F. MOW AT A HEIGHT OF 4-6 INCHES WHEN THE OATS SET SEED HEADS. MOW AT A HEIGHT OF 4 - 6 INCHES ONCE A MONTH OR WHENEVER WEED GROWTH REACHES 10 INCHES FOR THE REMAINDER OF THE FIRST SEASON.
- G. IF COOL SEASON WEED GROWTH IS HEAVY IN THE SPRING OF THE SECOND SEASON, MOW ONCE IN LATE MAY OF THE SECOND SEASON.



Ogen









EXTERIOR SIGN ELEVATIONS

07.26.2018

CEDAR FALLS, IA

FREESTANDING SIGNS

D/F INTERNALLY ILLUMINATED PYLON SIGN

D/F INTERNALLY ILLUMINATED MONUMENT SIGN

D/F INTERNALLY ILLUMINATED TENANT MONUMENT SIGN



185,000 SF YARD 60,000 SF HIGHWAY 20

> CLIENT APPROVAL DATE EST #: 4743-R1 07.26.18 WAM REVISE P.1, M.1 SIGN HEIGHTS, SQ. FT.; DELETE SOUTH & WEST ELE C-STORE SIGNS, ADD M.2 DATE: 06.25.2018 00.00.00 LANDLORD APPROVAL 00.00.00 DESIGNER: A. McKinney 00.00.00 XXXX SALES REP: N. Lison 00.00.00 XXXX 00.00.00 PROJ MGR: D. LaCrosse

RIDGEWAY AVENUE

M.2

0:4444444444

BUILDING A 10,000 SF

BUILDING B

BUILDING C 25,000 SF

Fleet 1 Farm

Fleet Farm Black Hawk County Cedar Falls, IA

SHEET NUMBER

DESIGN PHASE: CONCEPTUAL

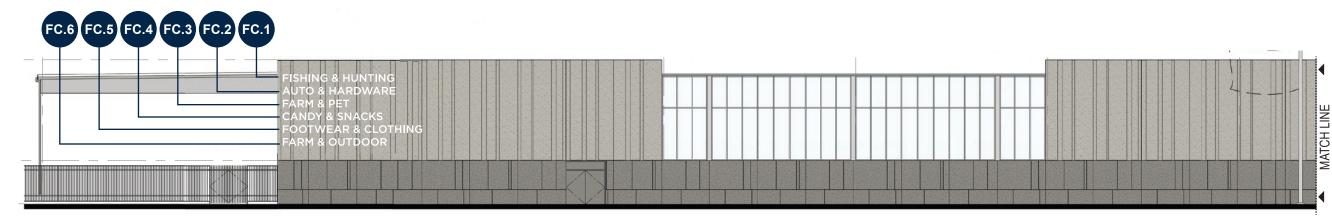
JONES SIGN

M.1

HIGHWAY 58

OUTLOT 2 1.53 AC±

OUTLOT 3 1.77 AC±



FRONT ELEVATION SCALE: 1" = 20'-0"



29-0"
NON-ILLUM. WHITE ACRYLIC LETTERS, 1/2" THICK SQUARE FOOTAGE: 45.9

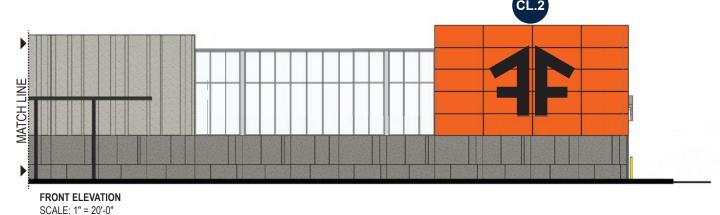
IFARM & OUTDOOR NON-ILLUM. WHITE ACRYLIC LETTERS, 1/2" THICK SQUARE FOOTAGE: 34.3



CUSTOM METAL SCREEN: PERFORATED METAL SCREEN BACKGROUND FOR CHANNEL LETTERS INTEGRATED INTO STOREFRONT DESIGN;







JONES SIGN
Your Vision. Accomplished.

WWW.JONESSIGN.COM

	ES
3N	DA
	DE
	SA
	PR

4740 D4	REV.	DATE	BY	DESCRIPTION
ST#: 4743-R1	1	07.26.18	WAM	REVISE P.1, M.1 SIGN HEIGHTS, SQ. FT.; DELETE SOUTH & WEST ELE C-STORE SIGNS, ADD M.2
	2	00.00.00	XX	XXXX
ATE: 06.25.2018	3	00.00.00	XX	XXXX
	4	00.00.00	XX	XXXX
ESIGNER: A. McKinney	5	00.00.00	XX	XXXX
,	6	00.00.00	XX	XXXX
ALES REP: N. Lison	7	00.00.00	XX	XXXX
ALLO INLI : IV. LIOOH	8	00.00.00	XX	XXXX
ROJ MGR: D. LaCrosse	9	00.00.00	XX	XXXX
RUJ WIGH. D. Laciosse	4.0	00 00 00	VV	VVVV

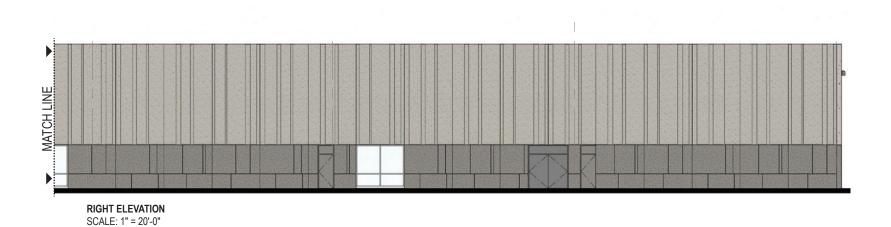
CLIENT APPROVAL	DATE	
LANDLORD APPROVAL	DATE	
QC		



eet Farm	
ack Hawk County edar Falls, IA	

SHEET NUMBER

This is an original, unpublished drawing by Jones Sign Co., Inc. It is for your personal use in conjunction with a project being planned for you by JONES SIGN. It is not to be shown to anyone outside of your organization, nor is it to be used, reproduced, copied or exhibited in any fashion. Use of this design or the salient elements of this design in any sign done by any other company, without the express written portion of this design or the salient elements of this design in any sign done by any other company, without the express written portion of this design or the salient elements of this design in any sign done by any other company, without the express written portion of this design or the salient elements of this design in any sign done by any other company, without the express written portion of this design or the salient elements of the salient elements elements of the salient elements element JONES SIGN, is forbidden by law and carriés a civil forfeiture of up to 25% of the purchase price of the sign. JONES SIGN will endeavor to closely match colors, including PMS, where specified. We cannot guarantee exact matches due to varying compatibility of surface materials and paints used. All sizes and dimensions are illustrated for client's



JONES SIGN Your Vision. Accomplished. WWW.JONESSIGN.COM

EST#: 4743-R1 DATE: 06.25.2018 DESIGNER: A. McKinney SALES REP: N. Lison

07.26.18 WAM REVISE P.1, M.1 SIGN HEIGHTS, SQ. FT.; DELETE SOUTH & WEST ELE C-STORE SIGNS, ADD M.2 00.00.00 XX XXXX
XX XXXX
XX XXXX
XX XXXX 00.00.00 00.00.00 PROJ MGR: D. LaCrosse

SCALE: 1" = 20'-0"

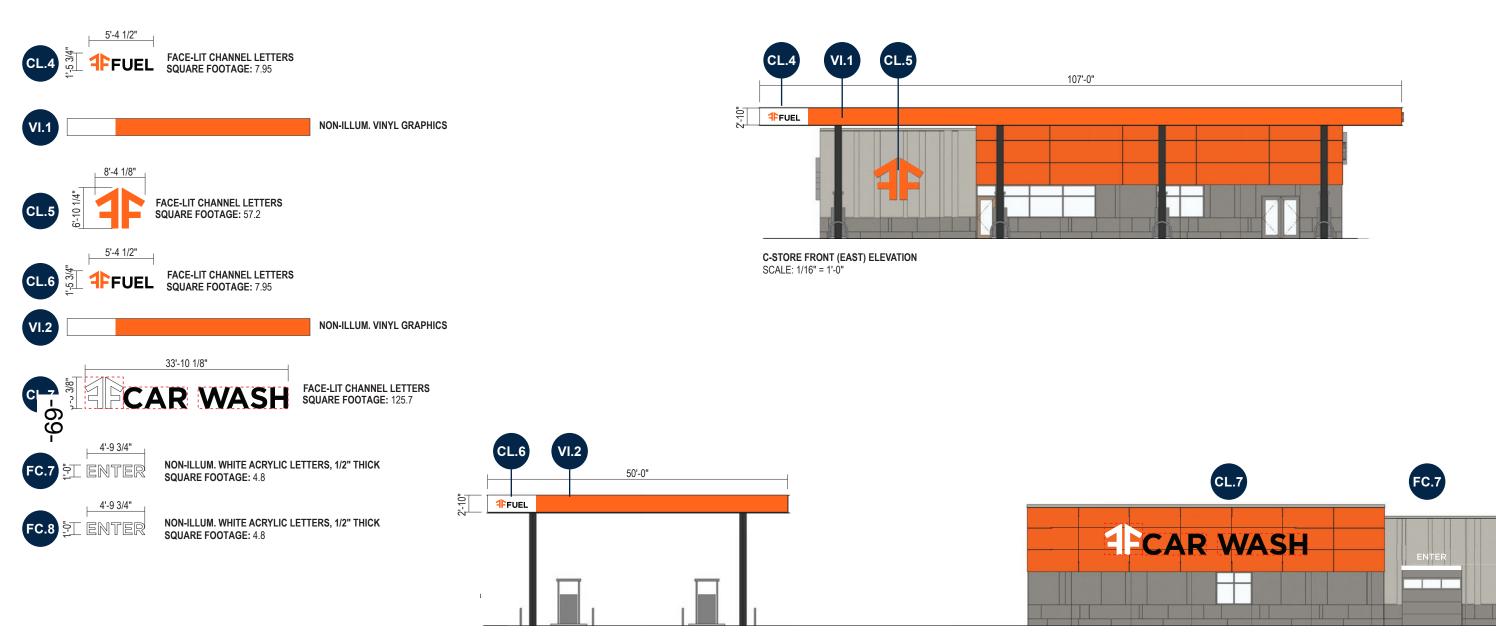
CLIENT APPROVAL LANDLORD APPROVAL QC



Fleet Farm Black Hawk County Cedar Falls, IA

SHEET NUMBER

DESIGN PHASE: CONCEPTUAL



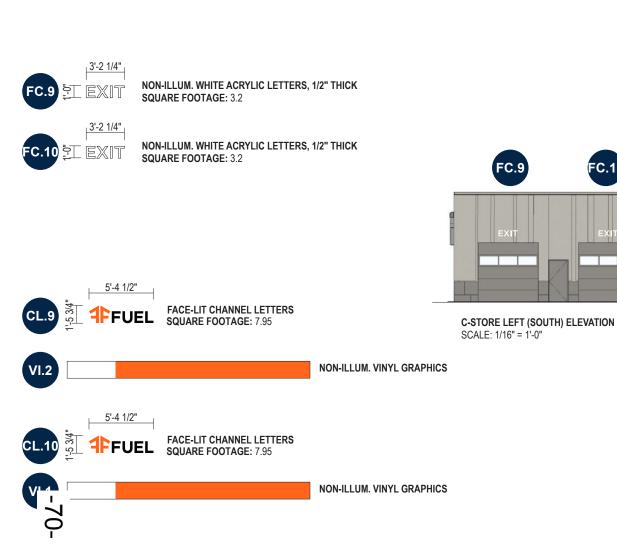
CANOPY RIGHT (NORTH) ELEVATION

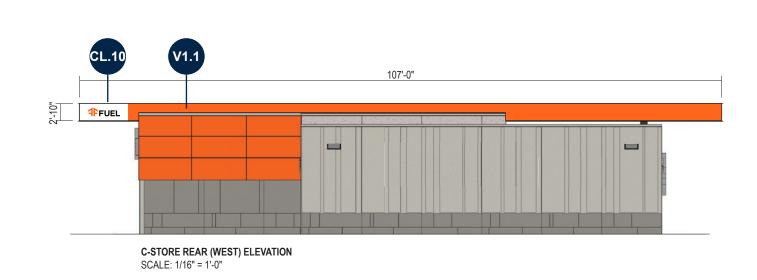
SCALE: 1/16" = 1'-0"



C-STORE RIGHT (NORTH) ELEVATION

SCALE: 1/16" = 1'-0"





1FUEL

CANOPY LEFT (SOUTH) ELEVATION

SCALE: 1/16" = 1'-0"





COLORS/FINISHES

P-1 MP BLACK, SATIN FINISH

P-2 MP TO MATCH PMS 165C, SATIN FINISH

V-2 3M 3630-84 TANGERINE TRANSLUCENT

V-3 3M 3635-222 DUAL COLOR FILM

P-3 MP TO MATCH BENJAMIN MOORE OC-65 CHANTILLY LACE

P-4 MP TO MATCH PRE-CAST WALL PANEL (T.B.D.)

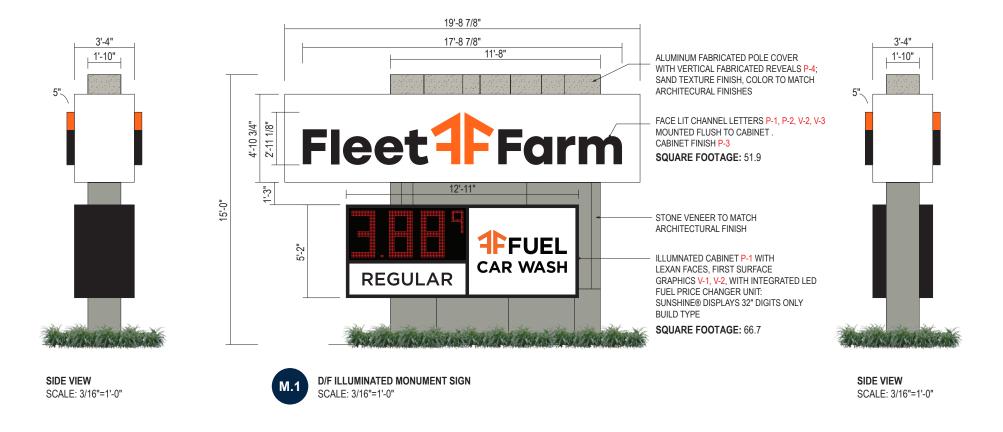
_	EST
JONES SIGN	DAT
Your Vision. Accomplished.	DES
WWW.JONESSIGN.COM	SAL
	DDC

4740 D4	REV.	DATE	BY	DESCRIPTION	CLIENT APPROVAL
EST#: 4743-R1	1	07.26.18	WAM	REVISE P.1, M.1 SIGN HEIGHTS, SQ. FT.; DELETE SOUTH & WEST ELE C-STORE SIGNS, ADD M.2	02.2.1171171.0712
	2	00.00.00	XX	XXXX	
DATE: 06.25.2018	3	00.00.00	XX	XXXX	
	4	00.00.00	XX	XXXX	LANDLORD APPROVAL
DESIGNER: A. McKinney	5	00.00.00	XX	XXXX	
· ·	6	00.00.00	XX	XXXX	
SALES REP: N. Lison	7	00.00.00	XX	XXXX	
	8	00.00.00	XX	XXXX	QC
PROJ MGR: D. LaCrosse	9	00.00.00	XX	XXXX	
TROO MORE B. Edologo	10	00.00.00	XX	XXXX	



Fleet Farm Black Hawk County Cedar Falls, IA

DESIGN PHASE: CONCEPTUAL





P-1 MP BLACK, SATIN FINISH

P-2 MP TO MATCH PMS 165C, SATIN FINISH

P-3 MP TO MATCH BENJAMIN MOORE OC-65 CHANTILLY LACE

P-4 MP TO MATCH PRE-CAST WALL PANEL (T.B.D.)

V-1 3M 3630-22 BLACK VINYL

V-2 3M 3630-84 TANGERINE V-3 3M 3635-222 DUAL COLOR FILM

JONES SIGN

Your Vision. Accomplished.

WWW.JONESSIGN.COM

EST #: **4743-R1** DATE: 06.25.2018

DESIGNER: A. McKinney SALES REP: N. Lison

REV. DATE BY DESCRIPTION 07.26.18 WAM REVISE P.1, M.1 SIGN HEIGHTS, SQ. FT.; DELETE SOUTH & WEST ELE C-STORE SIGNS, ADD M.2 00.00.00 00.00.00 XX XXXX 00.00.00 00.00.00 00.00.00 XX XXXXX 00.00.00 XXXX 00.00.00 XX 00.00.00 PROJ MGR: D. LaCrosse 00 00 00

CLIENT APPROVAL DATE LANDLORD APPROVAL QC



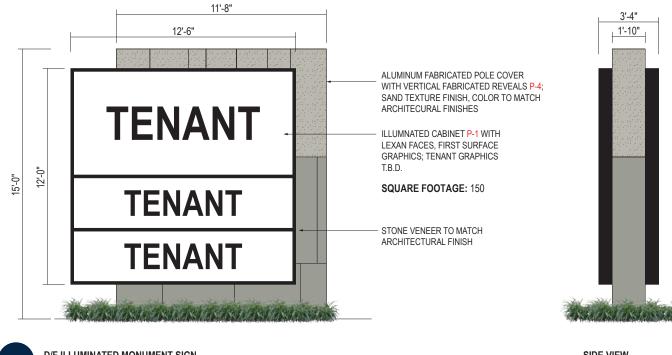
Fleet Farm Black Hawk County Cedar Falls, IA

SHEET NUMBER

DESIGN PHASE: CONCEPTUAL







D/F ILLUMINATED MONUMENT SIGN SCALE: 3/16"=1'-0"

SIDE VIEW SCALE: 3/16"=1'-0"

COLORS/FINISHES

P-1 MP BLACK, SATIN FINISH

P-3 MP TO MATCH BENJAMIN MOORE OC-65 CHANTILLY LACE

P-4 MP TO MATCH PRE-CAST WALL PANEL (T.B.D.)

_	EST
JONES SIGN	DAT
Your Vision. Accomplished.	DES
WWW.JONESSIGN.COM	SAL

EST#: 4743-R1	REV.	DATE	BY	DESCRIPTION
	1	07.26.18	WAM	REVISE P.1, M.1 SIGN HEIGHTS, SQ. FT.; DELETE SOUTH & WEST ELE C-STORE SIGNS, ADD M.2
	2	00.00.00	XX	XXXX
DATE: 06.25.2018	3	00.00.00	XX	XXXX
	4	00.00.00	XX	XXXX
DESIGNER: A. McKinney	5	00.00.00	XX	XXXX
220:0::2::::7::::::0;	6	00.00.00	XX	XXXX
SALES REP: N. Lison	7	00.00.00	XX	XXXX
	8	00.00.00	XX	XXXX
PROJ MGR: D. LaCrosse	9	00.00.00	XX	XXXX
FROJ WIGH. D. Laciosse	40	00 00 00	WW	2000/





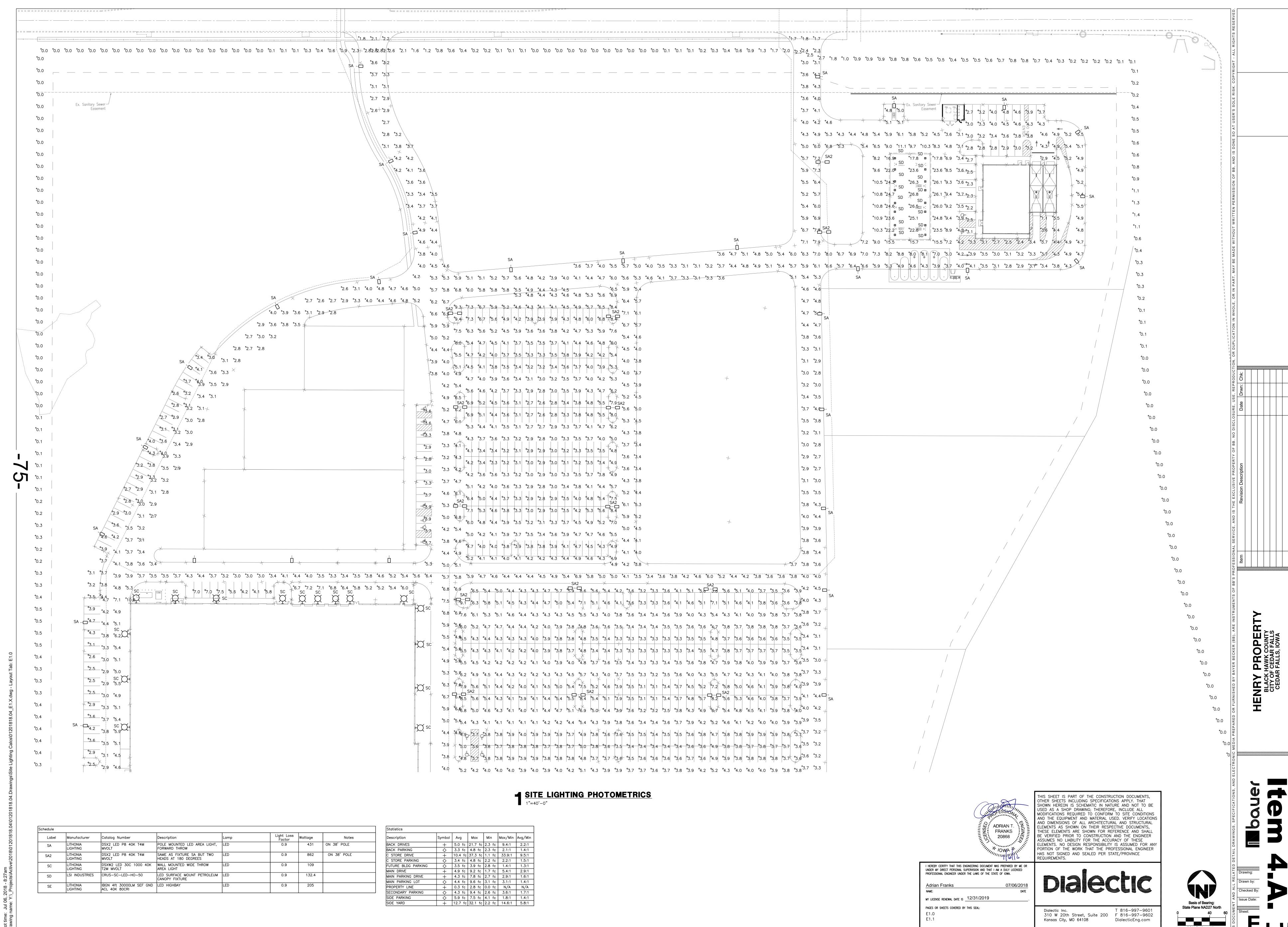
Fleet Farm Black Hawk County Cedar Falls, IA

DESIGN PHASE: CONCEPTUAL

QC

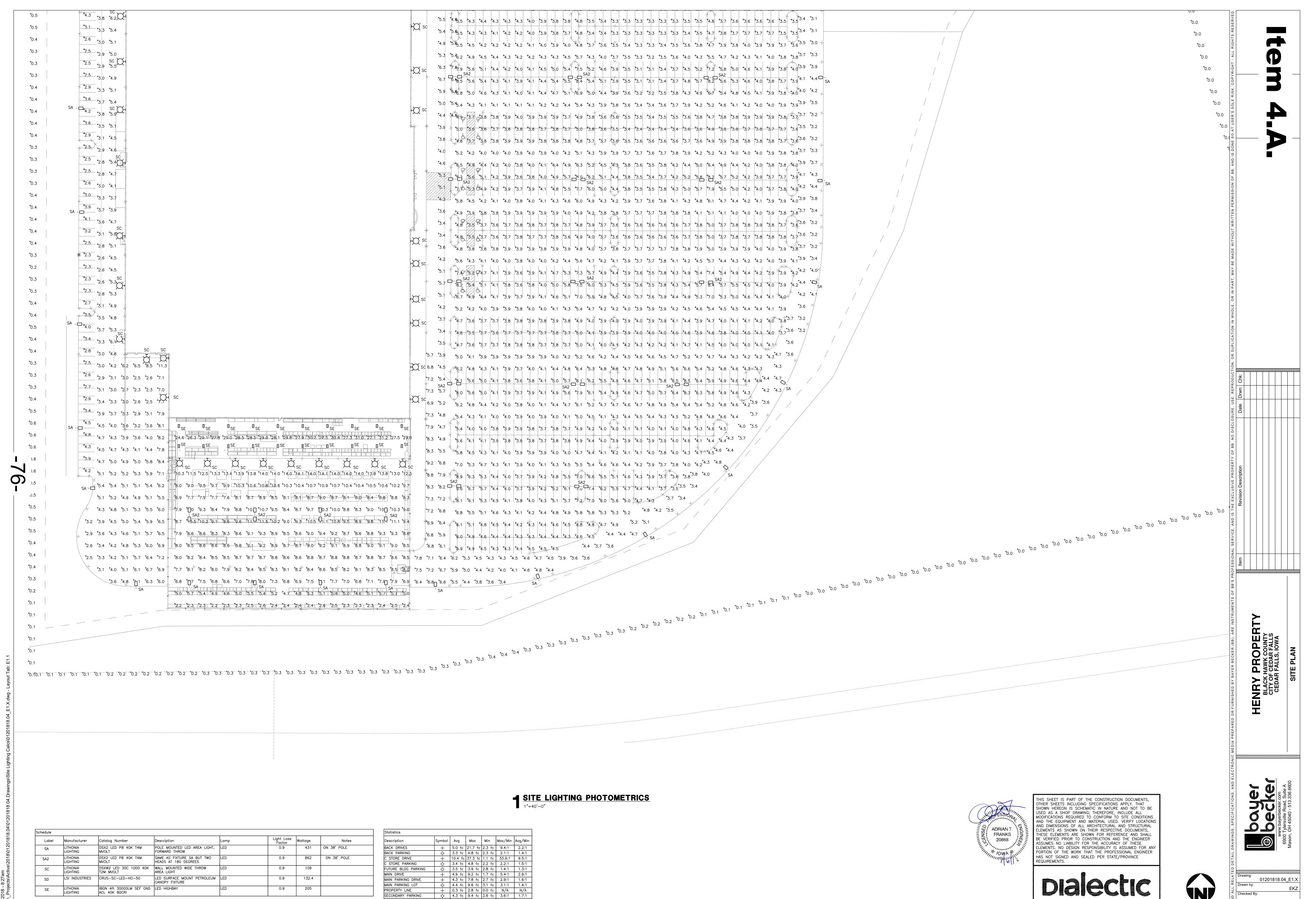
CLIENT APPROVAL

LANDLORD APPROVAL



SCALE: 1" = 40'

Copyright 2018



♦ 5.9 fc 7.5 fc 4.1 fc 1.8:1 1.4:1

+ 12.7 fc 32.1 fc 2.2 fc 14.6:1 5.8:1

SIDE PARKING

Basis of Bearing: State Plane NAD27 North SCALE: 1" = 40'

T 816-997-9601

DialecticEng.com

Copyright 2018

310 W 20th Street, Suite 200 F 816-997-9602

Dialectic Inc.

Kansas City, MO 64108

Issue Date: